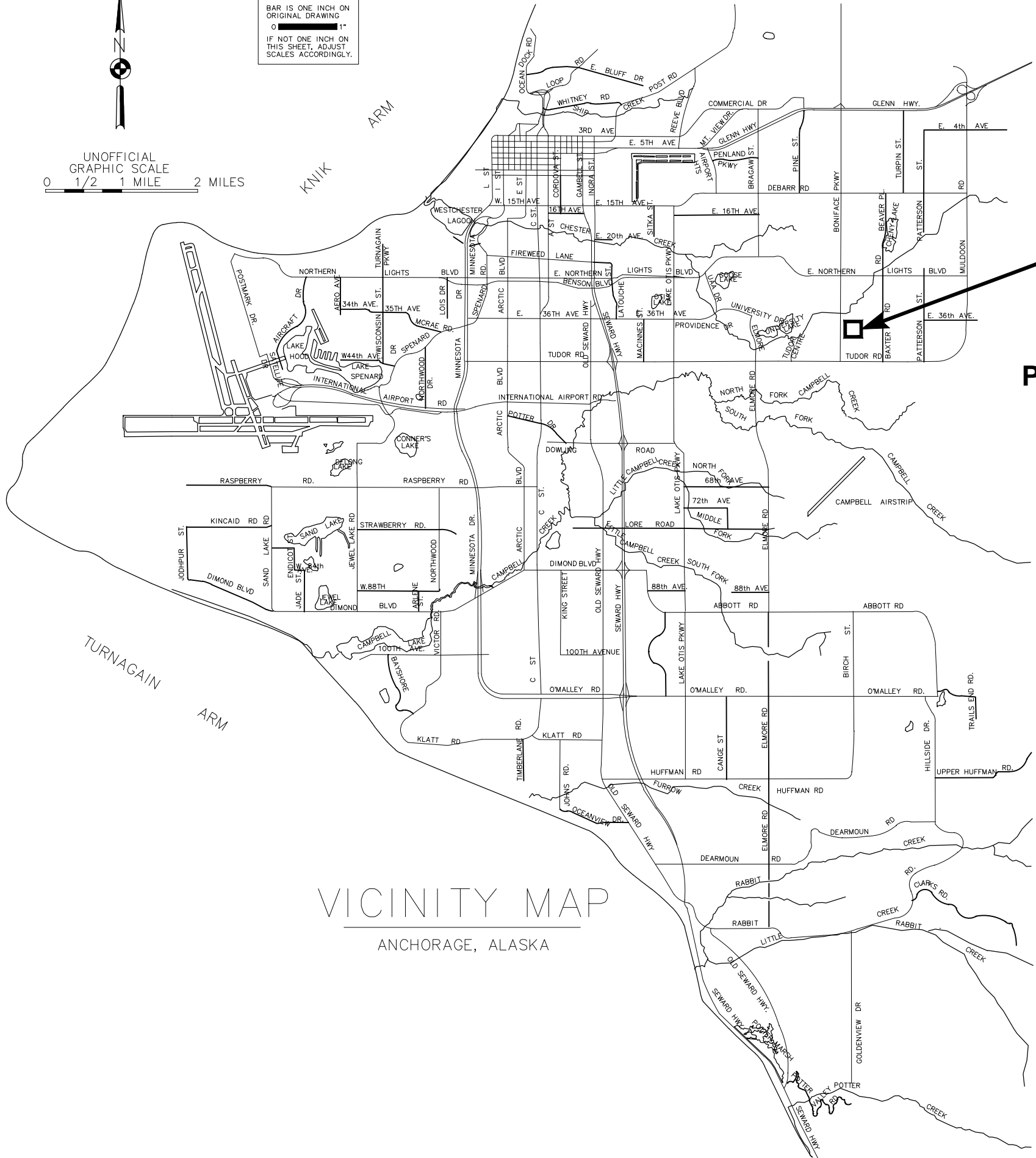


VERIFY SCALES
BAR IS ONE INCH ON ORIGINAL DRAWING
0 1" 1"
IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.



VICINITY MAP
ANCHORAGE, ALASKA



PROJECT AREA
THIS CONTRACT

**MUNICIPALITY OF ANCHORAGE
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT**

**IMAGE DRIVE / REFLECTION DRIVE AREA
ROAD RECONSTRUCTION - PHASE 1**

**PROJECT NUMBER: 14-50
JANUARY 2020**

95% DESIGN

PREPARED BY:



3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECL882-AK

APPROVED BY:

KENT KOHLHASE, P.E.
MUNICIPAL ENGINEER

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE MUNICIPALITY OF ANCHORAGE (MOA) STANDARD SPECIFICATIONS, DATED 2015, (HEREINAFTER REFERRED TO AS MASS), THE LATEST EDITION OF THE ANCHORAGE WATER AND WASTEWATER UTILITY (AWWU) DESIGN AND CONSTRUCTION PRACTICES MANUAL (DCPM) AND THE SPECIAL PROVISIONS.

2. THE LOCATION OF THE EXISTING FEATURES AND UTILITIES SHOWN IN THESE DRAWINGS ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF ALL UTILITIES ENCOUNTERED AND RECORD THEIR LOCATION ON THE CONTRACT RECORD DRAWINGS. DISCREPANCIES SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION OF THE ENGINEER.

3. PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS WHICH ARE NOT SPECIFICALLY INDICATED AS BEING PROVIDED BY THE OWNER IN THE SPECIAL PROVISIONS. THE PERMITS SHALL BE MAINTAINED ON THE PROJECT SITE. COPIES SHALL BE GIVEN TO THE ENGINEER.

4. ALL WORK IN CLOSE PROXIMITY TO EXISTING OVERHEAD TELEPHONE AND ELECTRIC UTILITIES SHALL COMPLY WITH APPLICABLE FEDERAL, STATE AND LOCAL STATUTES, CODES AND GUIDELINES AND THE CLEARANCE REQUIREMENTS OF THE SERVING UTILITY.

5. LIMITS OF ROADWAY EXCAVATION SHOWN ON THE DRAWINGS ARE APPROXIMATE. ACTUAL LIMITS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER DURING CONSTRUCTION OPERATIONS.

6. GEOTECHNICAL (SOILS) INFORMATION IS INCLUDED IN THE CONTRACT DOCUMENTS.

7. ALL WORK SHALL BE PERFORMED WITHIN PUBLIC RIGHT-OF-WAY, PUBLIC USE EASEMENT, SLOPE EASEMENT, TEMPORARY CONSTRUCTION EASEMENT, DRAINAGE EASEMENT, OR TEMPORARY CONSTRUCTION PERMIT AREAS. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL CONDITION, UNLESS OTHERWISE NOTED. REVEGETATION SHALL BE IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS.

8. CONTRACTOR SHALL RESTORE DISTURBED PROPERTY TO PRE-CONSTRUCTION CONDITIONS, UNLESS OTHERWISE DIRECTED BY ENGINEER. PAYMENT FOR RESTORING DISTURBED PROPERTY OUTSIDE OF IDENTIFIED CONSTRUCTION LIMITS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT SHALL BE MADE. DISTURBED AREAS NOT BEING PAVED SHALL BE TOPSOILED AND SEEDED WITH SCHEDULE A SEEDING MIX UNLESS OTHERWISE NOTED.

9. PROJECT CLEARING AND GRUBBING LIMITS SHALL COINCIDE WITH THE LIMITS OF DISTURBANCE AS SHOWN ON THE DEMOLITION (B) SHEETS. CONTRACTOR SHALL OBTAIN APPROVAL OF THE CLEARING AND GRUBBING LIMITS BY THE ENGINEER PRIOR TO CLEARING AND GRUBBING, SEE SPECIFICATIONS FOR MORE INFORMATION.

10. SLOPE LIMITS SHOWN ON THE DRAWINGS ARE APPROXIMATE. THE CONTRACTOR SHALL DETERMINE THE ACTUAL SLOPE LIMITS BASED ON PRECONSTRUCTION SURVEY DATA.

11. IN PREPARATION FOR AND IMMEDIATELY PRIOR TO PAVING, THE CONTRACTOR SHALL SAW CUT AND REMOVE ADDITIONAL PAVEMENT BEYOND THE INITIAL SAW CUT, A MINIMUM OF 1-FOOT ONTO UNDISTURBED ASPHALT. AT TRANSVERSE JOINTS FINAL SAW CUT LINE SHALL BE SKEWED 15° - 25° PER DETAIL 3, SHEET D3. TACK COAT SHALL BE APPLIED TO THE SAWN FACE OF ASPHALT PRIOR TO BEGINNING PAVING.

12. PAVEMENT CROSS SLOPE ON SIDE STREETS SHALL VARY AT INTERSECTIONS TO PROVIDE POSITIVE DRAINAGE. SEE ROADWAY (R) SHEETS FOR INTERSECTION LAYOUTS.

13. ALL WORK AND MATERIALS REQUIRED FOR REMOVING ANY LITTER OR DEBRIS CREATED BY CONSTRUCTION OPERATIONS WITHIN THE PROJECT LIMITS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT WILL BE MADE.

14. ALL ORGANIC MATERIAL SHALL BE REMOVED FROM THE SUBGRADE TO A DEPTH TO BE DETERMINED BY THE ENGINEER. NO ORGANIC MATERIAL OR OTHER DELETERIOUS MATERIAL SHALL BE UTILIZED FOR BACKFILL.

15. THE CONTRACTOR SHALL SUBMIT RECORD SURVEY NOTES WITH THE RECORD DRAWINGS.

16. EXCAVATION SHALL BE MEASURED BY EXCAVATED CROSS-SECTION AND SHALL BE LIMITED TO THE PAY LIMITS IDENTIFIED IN THE TYPICAL CROSS SECTIONS, UNLESS ADDITIONAL EXCAVATION IS DIRECTED BY THE ENGINEER.

17. THE PROJECT CENTERLINE STATIONING IS RIGHT-OF-WAY CENTERLINE PER SURVEY CONTROL DRAWINGS UNLESS OTHERWISE NOTED. SEE SURVEY CONTROL DRAWINGS FOR HORIZONTAL AND VERTICAL CONTROL.

18. THE EASEMENTS AND TEMPORARY CONSTRUCTION PERMITS ACQUIRED FOR THIS PROJECT MAY HAVE RESTRICTIONS. SEE CONTRACT DOCUMENTS FOR RESTRICTIONS.

19. ALL CURB LOCATIONS, RADIUS MEASUREMENTS AND ELEVATIONS ARE TO THE TOP BACK OF CURB (TBC) UNLESS OTHERWISE NOTED.

20. FURNISH AND INSTALL 4" PIPE INSULATION BOARD (R-20) BETWEEN THE STORM DRAIN IMPROVEMENTS AND THE WATER AND SEWER UTILITIES WHEN THE VERTICAL CLEARANCE IS LESS THAN THREE FEET. IF 18 INCHES OF VERTICAL SEPARATION BETWEEN WATER AND SEWER/STORM DRAINS CAN NOT BE MAINTAINED THEN WATER RELOCATION WILL BE REQUIRED WHEN DIRECTED BY THE ENGINEER. SEWER/STORM DRAIN PIPE JOINTS SHALL BE PLACED AT LEAST NINE (9) FEET FROM A WATER CROSSING.

21. EXISTING WATER AND SEWER SERVICE LINES ARE NOT SHOWN IN THE PROFILES UNLESS SPECIFICALLY CALLED OUT.

22. WATER RESULTING FROM THE CONTRACTOR'S DEWATERING EFFORT MAY NOT BE PUMPED OR OTHERWISE DIVERTED INTO EXISTING CREEKS, STORM DRAINS OR SANITARY SEWERS UNLESS REQUIRED PERMITS, INCLUDING, BUT NOT LIMITED TO, THE MUNICIPALITY OF ANCHORAGE STORM WATER PLAN REVIEW OFFICE, AWWU, AND THE ALASKA DEPARTMENT OF ENVIRONMENTAL CONSERVATION, ARE OBTAINED BY THE CONTRACTOR. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE ALLOWED TO DIVERT WATER FROM EXCAVATION ONTO ROADWAYS. THE CONTRACTOR SHALL PROVIDE DISPOSAL SITE FOR EXCESS WATER AND SHALL BE RESPONSIBLE FOR SECURING ALL NECESSARY PERMITS AND APPROVALS. THE CONTRACTOR SHALL PROVIDE COPIES OF PERMITS AND APPROVALS TO THE ENGINEER AND MOA ROW PERMIT OFFICE PRIOR TO BEGINNING DEWATERING. DEWATERING WORK SHALL BE IN ACCORDANCE WITH THE CONTRACTOR'S APPROVED DEWATERING PLAN.

23. ALL CURB AND GUTTER SHALL BE PAID AS "P.C.C. CURB AND GUTTER (ALL TYPES)".

24. EXISTING UTILITIES AND PROPOSED UTILITIES ARE SCHEMATICALLY SHOWN IN THE TYPICAL CROSS SECTIONS AND ARE AT AN ASSUMED APPROXIMATE LOCATION.

25. THE MATCH EXISTING ELEVATIONS AS SHOWN IN THE PLANS ARE APPROXIMATE. CONTRACTOR SHALL ADJUST PROPOSED GRADES AS REQUIRED TO MATCH INTO EXISTING ELEVATIONS PER THE DIRECTION OF THE ENGINEER.

26. ALL FILL, USABLE EXCAVATION, PLACED NATIVE MATERIALS AND TRENCH BACKFILL SHALL BE COMPACTED TO NINETY-FIVE PERCENT (95%) OF MAXIMUM DENSITY AT OPTIMUM MOISTURE CONTENT, PER MASS DIVISION 20 EARTHWORK, BASED ON MODIFIED PROCTOR TEST VALUES. ALL FILLS SHALL BE PLACED IN LIFTS NOT EXCEEDING 12 INCHES, UNLESS OTHERWISE NOTED.

27. FIRE HYDRANTS WILL BE ADJUSTED TO FINAL GRADE BY AWWU O&M DIVISION ON A REIMBURSABLE BASIS. THE CONTRACTOR IS TO PROVIDE WRITTEN NOTICE TO THE ENGINEER A MINIMUM OF SEVEN (7) DAYS PRIOR TO THE NEED FOR FINAL FIRE HYDRANT ADJUSTMENT. THE WRITTEN NOTICE IS TO CONTAIN, AT A MINIMUM, THE MANUFACTURER AND MODEL NUMBER OF THE HYDRANT AND VERTICAL ADJUSTMENT NEEDED IN SIX (6") INCREMENTS.

28. UNLESS OTHERWISE NOTED, ALL VALVE BOXES, CLEANOUTS, AND MANHOLES WITHIN THE CONSTRUCTION DISTURBANCE LIMITS SHALL BE ADJUSTED RELATIVE TO FINISH GRADE PER MASS OR THESE DRAWINGS.

CALL BEFORE YOU DIG!!!	
Alaska Digline, Inc. Statewide	811
Alaska Railroad	265-2520
Military Fuel Lines	552-3760
State Storm Drains	333-2411

[illegible]

File: J:\JobsData\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\01 Civil\Phase 1\10133.00 General Notes - Phase 1.dwg

1. EXISTING FEATURES & EASEMENTS ARE NOT SHOWN FOR CLARITY.
2. NOT ALL SHEETS ARE CALLED OUT FOR CLARITY.



1. DATA PROVIDED BY: _____ TITLE: _____
 THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION
 OF THE PROJECT AS CONSTRUCTED.
 CONTRACTOR: _____
 BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT
 SUPERVISION), THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
 DATA TRANSFER CHECKED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____
 BY: _____

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14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1		ALL
KEY MAP			
SCALE	HOR. 1"=60' VER. N/A	GRID SW1638, SW1738 DATE JAN 2020 STATUS 95%	SHEET G5 of G5

PARCEL 8
T13N R32 SEC 26

Horizontal Control

Coordinate System:
This project is located entirely within the Anchorage Bowl 2000 adjustment, a local surface grid coordinate system expressed in U.S. Survey feet units developed by the Alaska Department of Transportation.

Basis of Coordinates:
The Basis of Coordinates is NGS Station O'Malley, located near the intersection of the New Seward Highway and O'Malley Road. Said station has Anchorage Bowl 2000 coordinates of 303939.2310 N, 353362.5446 E. U.S. Survey Feet.

Basis of Bearings:
The Basis of Bearings is a local plane bearing between NGS Station O'Malley and NGS Station Loop 2 USE RM 3 1964. NGS Station Loop 2 USE RM 3 1964 bears N 01°43'26.4" E a distance of 49488.4476 feet from NGS Station O'Malley. NGS Station Loop 2 USE RM 3 1964 has Anchorage Bowl 2000 coordinates of 353405.2778 N, 354851.3982 E. U.S. Survey Feet.

Translation Parameters:
To convert the local coordinates to NAD83 (92) State Plane coordinates expressed in U.S. Survey Feet, translate using +2,296,868.6878 N U.S. Survey Feet, +1,312,517.4904 E U.S. Survey Feet, and scale using 0.9998910192.

Vertical Control

Vertical control is based on the MOA Benchmark GAAB-66, Elevation = 238.10 feet (GAAB), as described on page D-24 of the MOA Benchmark Book, and MOA Benchmark GAAB-96, Elevation = 313.83 feet (GAAB), as described on page D-26 of the MOA Benchmark Book.

Horizontal Control – Reflection Drive Alignment

Point	Station	Offset	Northing	Easting	Description
36	10+35.39	16.63 Rt	329447.41	367660.82	Set 1-1/8" Copper Survey Marker in sidewalk
649	10+68.97	36.82 Lt	329454.12	367724.19	Found 5/8" Rebar 0.45' below grade
650	10+88.76	53.84 Lt	329443.85	367751.21	Found 5/8" Rebar 0.45' below grade
651	10+91.29	89.58 Lt	329459.23	367783.65	Found 5/8" Rebar under wood fence
601	11+13.53	0.09 Lt	329394.40	367716.72	Found 1-1/2" Aluminum Cap 0.05' above pavement
635	11+23.68	0.20 Lt	329385.18	367720.96	Found 5/8" Rebar 0.08' below pavement
35	11+30.71	31.59 Lt	329390.93	367752.61	Set 1-1/8" Copper Survey Marker in sidewalk
659	11+93.77	29.67 Rt	329308.98	367720.75	Found 5/8" Rebar, slightly loose and bent
34	12+39.10	16.25 Rt	329272.46	367750.76	Set 1-1/8" Copper Survey Marker in top back of curb
634	13+06.22	0.06 Lt	329217.00	367791.94	Found 5/8" Rebar 0.05' below pavement
698	13+47.76	30.11 Rt	329168.11	367778.65	Found 5/8" Rebar 0.5' below grade, bent slightly

Horizontal Control – Image Drive Alignment

Point	Station	Offset	Northing	Easting	Description
601	40+00.09	0.19 Rt	329394.40	367716.72	Found 1-1/2" Aluminum Cap 0.05' above pavement
35	40+31.07	18.63 Rt	329390.93	367752.61	Set 1-1/8" Copper Survey Marker in sidewalk
649	40+32.30	50.65 Lt	329454.12	367724.19	Found 5/8" Rebar 0.45' below grade
650	40+52.36	29.85 Lt	329443.85	367751.21	Found 5/8" Rebar 0.45' below grade
651	40+88.25	29.93 Lt	329459.23	367783.65	Found 5/8" Rebar under wood fence
652	41+33.41	29.96 Lt	329478.49	367824.49	Found 5/8" Rebar 0.4' below grade, bent
653	41+95.15	30.10 Rt	329450.48	367905.93	Found 5/8" Rebar 0.5' above grade
4	41+96.18	14.56 Lt	329491.32	367887.84	Set 1-1/8" Copper Survey Marker in lip of curb
654	42+06.63	30.00 Rt	329455.46	367916.27	Found 5/8" Rebar 0.25' below grade
668	42+67.88	29.90 Lt	329526.75	367961.64	Found 5/8" Rebar 0.4' below grade
5	43+46.93	18.92 Rt	329478.29	368041.74	Set 1-1/8" Copper Survey Marker in top back of curb
602	43+77.26	50.06 Rt	329447.26	368072.16	Found 1-1/2" Aluminum Cap 0.05' above pavement

LEGEND

- Existing Aluminum Cap
- Existing Rebar or Iron Pipe
- Control set by CRW
- Control Point Number



RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
CONTRACTOR: _____ DATE: _____
BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____
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DATA TRANSFER CHECKED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____
BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
	GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
STAKING							
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							

GRAPHIC SCALE: 60 30 0 30 60

CRW ENGINEERING GROUP LLC

3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3253
#AEC0882-AK



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

14-50 IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1 ALL

SURVEY CONTROL SHEET

REFLECTION DR STA 9+00 TO STA 13+50
& IMAGE DR STA 40+00 TO STA 44+00

SCALE HOR. 1"=30'
VER. N/A

GRID SW1638, SW1738
DATE JAN 2020 STATUS 95% SHEET V1 of V2

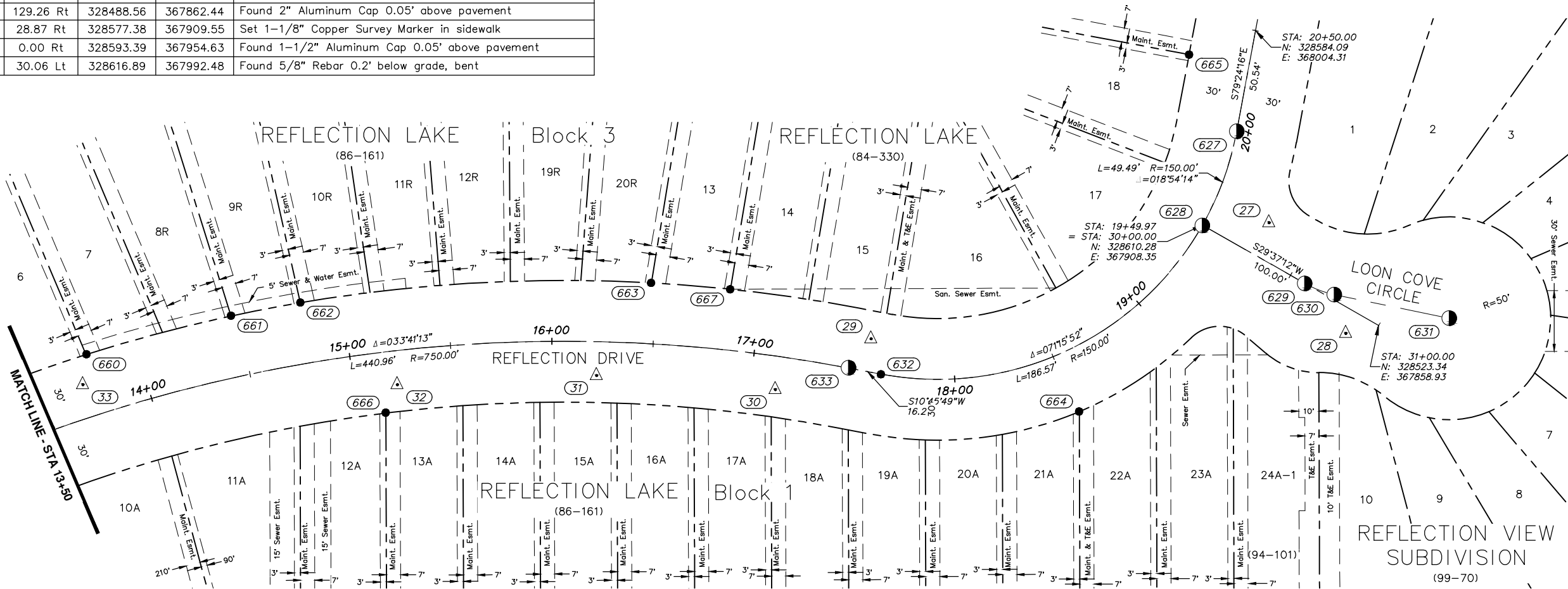


Horizontal Control – Reflection Drive Alignment					
Point	Station	Offset	Northing	Easting	Description
33	13+69.88	16.34 Lt	329162.42	367829.69	Set 1–1/8" Copper Survey Marker in top back of curb
660	13+75.93	30.07 Lt	329160.69	367844.67	Found 5/8" Rebar 0.3' below grade
661	14+46.92	30.17 Lt	329089.38	367863.72	Found 5/8" Rebar 0.5' below grade
662	14+80.35	30.05 Lt	329055.21	367870.17	Found 5/8" Rebar 0.5' below grade
666	15+14.15	29.81 Rt	329013.10	367815.86	Found 5/8" Rebar 0.45' below grade
32	15+21.77	16.78 Rt	329007.29	367829.66	Set 1–1/8" Copper Survey Marker in sidewalk
31	16+22.45	16.72 Rt	328909.04	367834.19	Set 1–1/8" Copper Survey Marker in sidewalk
663	16+47.28	29.95 Lt	328882.23	367879.87	Found 5/8" Rebar 0.3' below grade, leaning slightly
667	16+84.94	29.92 Lt	328843.20	367876.71	Found 5/8" Rebar 0.2' below grade, slightly loose and bent
30	17+12.92	16.39 Rt	328820.90	367827.29	Set 1–1/8" Copper Survey Marker in top back of curb
633	17+47.11	0.15 Lt	328784.82	367838.04	Found 1–1/2" Aluminum Cap 0.15' below pavement
29	17+55.40	16.28 Lt	328773.66	367852.33	Set 1–1/8" Copper Survey Marker in top back of curb
632	17+63.39	0.08 Rt	328768.87	367834.78	Found 5/8" Rebar 0.1' below pavement
664	18+51.30	29.84 Rt	328671.11	367816.44	Found 5/8" Rebar 0.25' below grade
28	19+42.65	88.14 Rt	328539.53	367854.98	Set 1–1/8" Copper Survey Marker in top back of curb
629	19+49.78	58.02 Rt	328559.91	367879.56	Found 2" Aluminum Cap 0.05' above pavement
628	19+49.79	0.05 Rt	328610.32	367908.17	Found 2" Aluminum Cap 0.05' below pavement
630	19+51.42	73.44 Rt	328545.30	367874.08	Found 2" Aluminum Cap 0.05' above pavement
631	19+60.71	129.26 Rt	328488.56	367862.44	Found 2" Aluminum Cap 0.05' above pavement
27	19+64.45	28.87 Rt	328577.38	367909.55	Set 1–1/8" Copper Survey Marker in sidewalk
627	19+99.46	0.00 Rt	328593.39	367954.63	Found 1–1/2" Aluminum Cap 0.05' above pavement
665	20+32.34	30.06 Lt	328616.89	367992.48	Found 5/8" Rebar 0.2' below grade, bent

Horizontal Control – Loon Cove Circle Alignment					
Point	Station	Offset	Northing	Easting	Description
628	30+00.05	0.25 Rt	328610.32	367908.17	Found 2" Aluminum Cap 0.05' below pavement
27	30+27.99	17.25 Lt	328577.38	367909.55	Set 1–1/8" Copper Survey Marker in sidewalk
629	30+58.02	0.17 Rt	328559.91	367879.56	Found 2" Aluminum Cap 0.05' above pavement
630	30+73.42	2.30 Lt	328545.3	367874.08	Found 2" Aluminum Cap 0.05' above pavement
28	30+87.89	11.44 Rt	328539.53	367854.98	Set 1–1/8" Copper Survey Marker in top back of curb

LEGEND

- Existing Aluminum Cap
- Existing Rebar or Iron Pipe
- Control set by CRW
- Control Point Number



RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

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PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

GRAPHIC SCALE					
60	30	0	30	60	
FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV
DESIGN CRW Books 147, 148, & 151		GAAB 66	See page D-24 of the MOA Benchmark Book	238.10	
		GAAB 96	See page D-26 of the MOA Benchmark Book	313.83	
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ASBUILT					
CONTRACTOR					
INSPECTOR					
BASIS OF THIS DATUM GAAB 1972 ADJUST					
REVISIONS					

CRW ENGINEERING GROUP LLC

3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AEC0882-AK

STATE OF ALASKA
49 TH
Michael L. Jokela
LS-7839
REGISTERED PROFESSIONAL LAND SURVEYOR

MUNICIPALITY OF ANCHORAGE

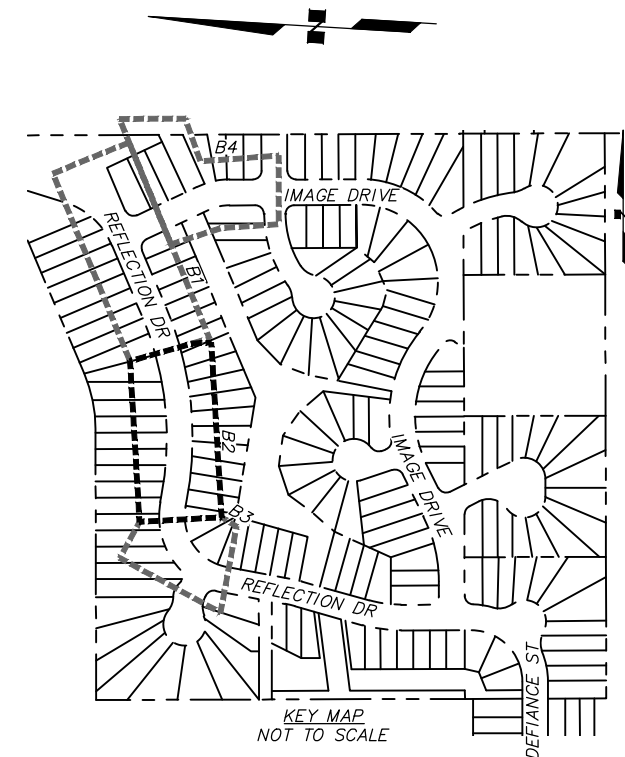
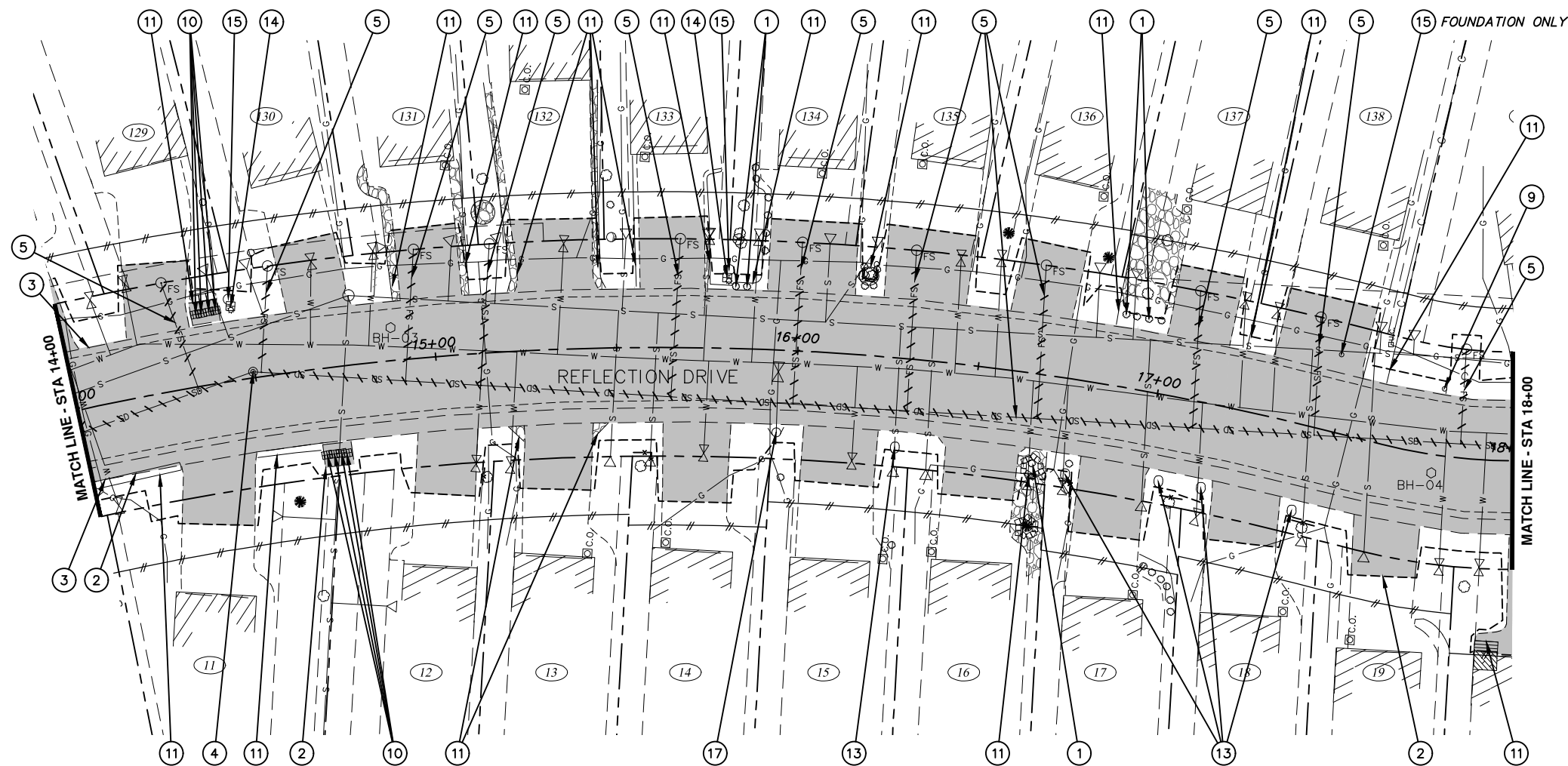
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

14-50 IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION – PHASE 1 ALL



SURVEY CONTROL SHEET




REFLECTION DRIVE STA 13+50 TO STA 20+50 & LOON COVE CIRCLE

SCALE HOR. 1"=30' VER. N/A GRID SW1638, SW1738 DATE JAN 2020 STATUS 95% SHEET V2 of V2



NOTES:

- ① CLEAR AND GRUB WITHIN LIMITS OF DISTURBANCE AFTER CLEARING LIMITS HAVE BEEN APPROVED AND AFTER TEMPORARY TREE PROTECTION FENCES (SECTION 75.12) HAVE BEEN ESTABLISHED AS SHOWN, OR AS DIRECTED BY THE ENGINEER IN THE FIELD (SECTION 20.04). NOT ALL TREES, SHRUBS, AND VEGETATION ARE SPECIFICALLY CALLED OUT OR SHOWN.
 - ② REMOVE SIDEWALK OR CONCRETE APRON (SECTION 20.07).
 - ③ REMOVE CURB AND GUTTER (SECTION 20.08).
 - ④ REMOVE MANHOLE OR CATCH BASIN (SECTION 55.11).
 - ⑤ REMOVE PIPE (SECTION 70.07).
 - ⑨ REMOVE AND RELOCATE SIGN (SECTION 70.11).
 - ⑩ RELOCATE MAILBOX OR CLUSTER MAILBOX UNIT (SECTION 70.17).
 - ⑪ REMOVAL/DISPOSAL AND/OR SALVAGE/INSTALLATION OF OBSTRUCTIONS (SECTION 70.22), SEE NOTE 3.
 - ⑬ SALVAGE AND RELOCATE EXISTING BOULDERS OR DISPOSE OF BOULDERS AS DIRECTED IN THE FIELD BY ENGINEER (SECTION 75.11).
 - ⑭ REMOVE JUNCTION BOX (SECTION 80.08).
 - ⑮ REMOVE LUMINAIRE POLE (SECTION 80.28).
 - ⑰ PROTECT IN PLACE
 -  REMOVAL OF PAVEMENT (SECTION 20.09) AND/OR, SIDEWALK, CURB & GUTTER, AND CONCRETE, AS SHOWN & NOTED IN SUMMARY TABLES.
 - APPROXIMATE LIMITS OF DISTURBANCE
 - \ \ \ REMOVE PIPE
 -  TEMPORARY TREE PROTECTION FENCE (SECTION 75.12), LOCATIONS TO BE FIELD VERIFIED, SEE DETAIL ON SHEET B3.

RECORD DRAWING 1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ BY: _____ TITLE: _____ DATE: _____ 2. DATA TRANSFERRED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ 3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ BY: _____			<table border="1"> <tr> <th>DATA</th> <th>DRAWN BY</th> <th>CHECKED BY</th> </tr> <tr> <td>BASE</td> <td>TS</td> <td>MJ</td> </tr> <tr> <td>TOPOGRAPHY</td> <td>TS</td> <td>MJ</td> </tr> <tr> <td>PROFILE</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>STORM SEWER</td> <td>MV</td> <td>JH</td> </tr> <tr> <td>WATER/SANITARY SEWER</td> <td>MV</td> <td>JK</td> </tr> <tr> <td>GAS</td> <td>MV</td> <td>JK</td> </tr> <tr> <td>TELEPHONE</td> <td>MV</td> <td>JK</td> </tr> <tr> <td>ELECTRIC</td> <td>JH</td> <td>TK</td> </tr> <tr> <td>DESIGN</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>QUANTITIES</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>PRELIMINARY/FINAL</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>MUNICIPAL/STATE</td> <td>RB</td> <td>JK</td> </tr> </table>			DATA	DRAWN BY	CHECKED BY	BASE	TS	MJ	TOPOGRAPHY	TS	MJ	PROFILE	RB	JK	STORM SEWER	MV	JH	WATER/SANITARY SEWER	MV	JK	GAS	MV	JK	TELEPHONE	MV	JK	ELECTRIC	JH	TK	DESIGN	RB	JK	QUANTITIES	RB	JK	PRELIMINARY/FINAL	RB	JK	MUNICIPAL/STATE	RB	JK	<div style="text-align: center;"> 40 20 0 20 40 GRAPHIC SCALE </div> <table border="1"> <thead> <tr> <th>FIELD BOOKS</th> <th>BM NO.</th> <th>LOCATION</th> <th>ELEV.</th> <th>REV</th> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> </tr> </thead> <tbody> <tr> <td>DESIGN CRW Books 147, 148, & 151</td> <td>GAAB 66</td> <td>See page D-24 of the MOA Benchmark Book</td> <td>238.10</td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td></td> <td>GAAB 96</td> <td>See page D-26 of the MOA Benchmark Book</td> <td>313.83</td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>STAKING</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>ASBUILT</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>CONTRACTOR</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>INSPECTOR</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td colspan="8">BASIS OF THIS DATUM GAAB 1972 ADJUST</td> </tr> </tbody> </table>			FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10						GAAB 96	See page D-26 of the MOA Benchmark Book	313.83					STAKING								ASBUILT								CONTRACTOR								INSPECTOR								BASIS OF THIS DATUM GAAB 1972 ADJUST								<div style="display: flex; justify-content: space-between;"> <div style="text-align: center;">  CRW ENGINEERING GROUP LLC <small>3940 ARCTIC BLVD, SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AECLEB2-AK</small> </div> <div style="text-align: center;">  <small>JUSTIN T. KEENE CE-11775 REGISTERED PROFESSIONAL ENGINEER</small> </div> <div style="text-align: center;">  <small>MUNICIPALITY OF ANCHORAGE</small> </div> </div>			<div style="text-align: center;"> PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT 14-50 IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1 DEMOLITION PLAN STA 14+00 TO STA 18+00 </div>		
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File:z:\JobData\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\01 Civil\Phase 1\0133.00 Demolition Plan - Phase 1.dwg

20.07						
REMOVE SIDEWALK OR CONCRETE APRON						
SHEET	APPX STATION BEGIN	APPX OFFSET (FT)	APPX STATION END	APPX OFFSET (FT)	AREA (SY)	REMARKS
B1	9+82.9	30.7 RT	14+00.0	20.3 RT	188	REFLECTION DR SIDEWALK
B1	12+35.7	16.4 LT	12+55.4	16.4 LT	49	PARCEL 124 DRIVEWAY
B1	40+23.2	20.6 RT	41+00.0	20.6 RT	32	IMAGE DR SIDEWALK
B2	14+00.0	20.3 RT	18+00.0	20.4 RT	176	REFLECTION DR SIDEWALK
B2	14+39.0	20.0 LT	14+39.0	20.0 LT	5	REFLECTION DR MAILBOX PAD
B2	14+70.1	24.1 RT	14+70.1	24.1 RT	6	REFLECTION DR MAILBOX PAD
B2	17+60.8	20.6 RT	17+77.3	20.0 RT	31	PARCEL 19 DRIVEWAY
B3	18+00.0	20.4 RT	19+33.9	33.0 RT	71	REFLECTION DR SIDEWALK
B3	19+65.7	32.1 RT	20+05.0	20.5 RT	23	REFLECTION DR SIDEWALK
B4	41+00.0	20.6 RT	43+54.1	20.5 RT	108	IMAGE DR SIDEWALK
B4	41+84.5	21.7 LT	41+89.8	21.6 RT	4	IMAGE DR MAILBOX PAD
B4	61+62.5	20.6 RT	61+75.8	20.4 RT	4	MIRAGE CIRCLE SIDEWALK

20.08						
REMOVE CURB AND GUTTER						
SHEET	APPX STATION BEGIN	APPX OFFSET (FT)	APPX STATION END	APPX OFFSET (FT)	LENGTH (FT)	REMARKS
B1	9+90.0	16.4 LT	41+00.0	16.4 LT	191	REFLECTION DR/IMAGE DR
B1	9+90.0	16.5 RT	14+00.0	16.4 RT	399	REFLECTION DR
B1	14+00.0	16.3 LT	41+00.0	16.6 RT	349	REFLECTION DR/IMAGE DR
B2	14+00.0	16.3 LT	18+00.0	16.6 LT	404	REFLECTION DR
B2	14+00.0	16.4 RT	18+00.0	16.3 RT	396	REFLECTION DR
B3	18+00.0	16.6 LT	20+05.0	16.5 LT	183	REFLECTION DR
B3	18+00.0	16.3 RT	19+38.6	45.5 RT	173	REFLECTION DR/LOON COVE CIR
B3	19+61.3	45.6 RT	20+05.0	16.5 RT	68	REFLECTION DR/LOON COVE CIR
B4	41+00.0	16.4 LT	61+62.4	16.7 RT	281	IMAGE DR
B4	41+00.0	16.6 RT	43+62.1	36.1 RT	266	IMAGE DR




20.09						
REMOVE PAVEMENT						
SHEET	STATION TO STATION	OFFSET	AREA (SY)	REMARKS		
B1	BOP TO STA 14+00, STA 40+00 TO 41+00	LT & RT	2,296	RELFECTION DRIVE, IMAGE DRIVE, DRIVEWAYS		
B2	STA 14+00 TO STA 18+00	LT & RT	2,035	RELFECTION DRIVE, DRIVEWAYS		
B3	STA 18+00 TO STA 20+05	LT & RT	1,166	REFLECTION DRIVE, LOON COVE CIRCLE, DRIVEWAYS		
B4	STA 41+00 TO EOP	LT & RT	1,374	IMAGE DRIVE, DRIVEWAYS		

NOTES: 1. SEE ROADWAY IMPROVEMENT SHEETS FOR ROADWAY PAVEMENT REMOVAL LIMITS.
2. SEE DRIVEWAY RECONSTRUCTION TABLE FOR DRIVEWAY PAVEMENT REMOVAL LIMITS.

55.11					
REMOVE MANHOLE OR CATCH BASIN					
SHEET	APPX STATION	APPX OFFSET (FT)	CATCH BASIN	MANHOLE	REMARKS
B1	10+14.0	12.6 LT		X	
B1	11+48.6	15.6 LT	X		
B1	11+59.3	12.1 RT		X	
B1	11+62.2	15.7 RT	X		
B2	14+49.5	1.7 LT		X	
B3	18+42.7	15.8 RT	X		
B3	18+51.5	8.5 RT		X	
B3	18+66.3	15.4 LT	X		
B3	19+43.6	7.7 RT		X	
B3	30+33.3	14.3 RT	X		
B3	30+33.2	14.1 LT	X		
B4	42+05.9	15.6 LT	X		
B4	42+08.0	15.8 RT	X		
B4	42+39.9	6.8 LT		X	
B4	43+80.2	1.8 LT		X	

70.07							
REMOVE PIPE							
SHEET	APPX STA BEGIN	APPX OFFSET (FT)	APPX STA END	APPX OFFSET (FT)	SIZE (INCH)	LENGTH (FT)	REMARKS
B1	9+66.3	86.4 LT	10+13.9	12.6 LT	18	93	STORM DRAIN
B1	10+13.9	12.6 LT	11+59.3	12.1 RT	10	147	STORM DRAIN
B1	11+59.3	12.1 RT	11+48.6	15.6 LT	10	30	STORM DRAIN
B1	11+59.3	12.1 RT	11+62.2	15.6 RT	10	5	STORM DRAIN
B1	11+59.3	12.1 RT	14+49.5	1.7 LT	18	290	STORM DRAIN
B1	11+66.4	12.1 RT	11+66.4	30.0 LT	6	42	FOOTING DRAIN
B1	12+00.4	12.0 RT	12+00.4	30.0 LT	6	42	FOOTING DRAIN
B1	12+42.4	12.0 RT	12+42.4	30.0 LT	6	42	FOOTING DRAIN
B1	12+78.4	12.0 RT	12+78.4	30.0 LT	6	42	FOOTING DRAIN
B1	13+22.7	11.9 RT	13+21.8	30.2 LT	6	42	FOOTING DRAIN
B1	13+57.2	10.3 RT	13+54.5	30.0 LT	6	40	FOOTING DRAIN
B2	14+00.6	6.1 RT	13+96.2	30.0 LT	6	36	FOOTING DRAIN
B2	14+34.0	1.2 RT	14+28.9	30.0 LT	6	32	FOOTING DRAIN
B2	14+49.5	1.7 LT	18+51.5	8.5 RT	10	402	STORM DRAIN
B2	14+51.7	1.2 LT	14+57.5	30.0 LT	6	29	FOOTING DRAIN
B2	14+91.1	5.9 RT	14+96.4	30.0 LT	6	36	FOOTING DRAIN
B2	15+11.9	8.9 RT	15+16.6	30.0 LT	6	39	FOOTING DRAIN
B2	15+65.0	13.6 RT	15+67.4	30.0 LT	6	44	FOOTING DRAIN
B2	15+99.4	14.5 RT	15+99.8	30.0 LT	6	45	FOOTING DRAIN
B2	16+31.6	14.3 RT	16+30.1	30.0 LT	6	44	FOOTING DRAIN
B2	16+68.1	12.3 RT	16+64.8	30.0 LT	6	42	FOOTING DRAIN
B2	17+11.4	7.5 RT	17+06.3	30.0 LT	6	38	FOOTING DRAIN
B2	17+44.7	2.1 RT	17+38.9	30.0 LT	6	33	FOOTING DRAIN
B2	17+85.2	3.9 LT	17+83.8	30.0 LT	6	26	FOOTING DRAIN

NOTE: EXISTING HEAT TRACE AND/OR HEAT TRACE CONDUIT IS LOCATED WITHIN EXISTING STORM DRAIN & CULVERT PIPES. CONTRACTOR SHALL REMOVE AND DISPOSE OF EXISTING HEAT TRACE AND/OR HEAT TRACE CONDUIT. THIS WORK SHALL BE INCIDENTAL TO SECTION 70.07 PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.

RECORD DRAWING 1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ BY: _____ TITLE: _____ DATE: _____ 2. DATA TRANSFERRED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ 3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ BY: _____		<table><tr><th>DATA</th><th>DRAWN BY</th><th>CHECKED BY</th></tr><tr><td>BASE</td><td>TS</td><td>MJ</td></tr><tr><td>TOPOGRAPHY</td><td>TS</td><td>MJ</td></tr><tr><td>PROFILE</td><td>RB</td><td>JK</td></tr><tr><td>STORM SEWER</td><td>MV</td><td>JH</td></tr><tr><td>WATER/SANITARY SEWER</td><td>MV</td><td>JK</td></tr><tr><td>GAS</td><td>MV</td><td>JK</td></tr><tr><td>TELEPHONE</td><td>MV</td><td>JK</td></tr><tr><td>ELECTRIC</td><td>JH</td><td>TK</td></tr><tr><td>DESIGN</td><td>RB</td><td>JK</td></tr><tr><td>QUANTITIES</td><td>RB</td><td>JK</td></tr><tr><td>PRELIMINARY/FINAL</td><td>RB</td><td>JK</td></tr><tr><td>MUNICIPAL/STATE</td><td>RB</td><td>JK</td></tr></table>	DATA	DRAWN BY	CHECKED BY	BASE	TS	MJ	TOPOGRAPHY	TS	MJ	PROFILE	RB	JK	STORM SEWER	MV	JH	WATER/SANITARY SEWER	MV	JK	GAS	MV	JK	TELEPHONE	MV	JK	ELECTRIC	JH	TK	DESIGN	RB	JK	QUANTITIES	RB	JK	PRELIMINARY/FINAL	RB	JK	MUNICIPAL/STATE	RB	JK	<table><tr><th>FIELD BOOKS</th><th>BM NO.</th><th>LOCATION</th><th>ELEV.</th></tr><tr><td>DESIGN CRW Books 147, 148, & 151</td><td>GAAB 66</td><td>See page D-24 of the MOA Benchmark Book</td><td>238.10</td></tr><tr><td></td><td>GAAB 96</td><td>See page D-26 of the MOA Benchmark Book</td><td>313.83</td></tr></table> <table><tr><th>STAKING</th><th>ASBUILT</th><th>CONTRACTOR</th><th>INSPECTOR</th></tr><tr><td></td><td></td><td></td><td></td></tr></table> <table><tr><td colspan="4">BASIS OF THIS DATUM GAAB 1972 ADJUST</td></tr><tr><td></td><td></td><td></td><td></td></tr></table>	FIELD BOOKS	BM NO.	LOCATION	ELEV.	DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10		GAAB 96	See page D-26 of the MOA Benchmark Book	313.83	STAKING	ASBUILT	CONTRACTOR	INSPECTOR					BASIS OF THIS DATUM GAAB 1972 ADJUST								<table><tr><th>REV</th><th>DATE</th><th>DESCRIPTION</th><th>BY</th></tr><tr><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td></tr></table>	REV	DATE	DESCRIPTION	BY																																					<div><div>3940 ARCTIC BLVD, SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AEC1882-AK</div></div> <div></div> <div></div>	<table><tr><th colspan="4">PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT</th></tr><tr><td>14-50</td><td>IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1</td><td colspan="2" rowspan="2">ALL</td></tr><tr><th colspan="4">DEMOLITION SUMMARY</th></tr><tr><td>SCALE HOR. N/A VER. N/A</td><td>GRID SW1638, SW1738 DATE JAN 2020</td><td>STATUS 95%</td><td>SHEET B5 of B8</td></tr></table>	PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT				14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	ALL		DEMOLITION SUMMARY				SCALE HOR. N/A VER. N/A	GRID SW1638, SW1738 DATE JAN 2020	STATUS 95%	SHEET B5 of B8
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70.22

REMOVAL/DISPOSAL AND/OR SALVAGE/INSTALLATION OF OBSTRUCTIONS (11)

SHEET	APPX STATION	APPX OFFSET (FT)	OBSTRUCTION ITEM	QUANTITY	REMARKS
B1	9+68	113.9 LT	TRASH	10 CY	
B1	10+69	23.3 RT	LANDSCAPING ROCKS	65 SF	
B1	10+79	24.4 RT	PLANTER	3 SF	
B1	11+11	24.7 RT	PLANTER	3 SF	
B1	11+39	28.0 LT	LANDSCAPING ROCKS	298 SF	
B1	11+78	18.9 LT	LANDSCAPING ROCKS	53 SF	
B1	12+30	18.3 LT	LANDSCAPING ROCKS	107 SF	
B1	12+42	28.4 RT	LANDSCAPING ROCKS	34 SF	
B1	12+57	18.8 LT	LANDSCAPING ROCKS	41 SF	
B1	12+61	25.0 RT	LANDSCAPING ROCKS	26 SF	
B1	13+52	21.5 RT	PLANTERS	45 SF	
B1	13+93	21.5 RT	PLANTERS	27 SF	
B1	40+92	19.7 LT	PLANTERS	7 SF	
B1	40+98	19.7 LT	PLANTERS	7 SF	
B2	14+13	21.1 RT	PLANTERS	52 SF	
B2	14+38	26.2 LT	MODULAR CONCRETE WALL	10 LF	
B2	14+57	21.3 RT	PLANTERS	32 SF	
B2	14+91	26.0 LT	LANDSCAPING ROCKS	44 SF	
B2	15+09	26.5 LT	LANDSCAPING ROCKS	33 SF	
B2	15+22	23.6 RT	LANDSCAPING ROCKS	10 SF	
B2	15+22	27.0 LT	LANDSCAPING ROCKS	34 SF	
B2	15+44	23.7 RT	LANDSCAPING ROCKS	10 SF	
B2	15+45	24.7 LT	LANDSCAPING ROCKS	36 SF	
B2	15+56	23.8 LT	LANDSCAPING EDGING	23 SF	
B2	15+75	22.8 LT	LANDSCAPING EDGING	32 SF	
B2	15+90	22.3 LT	LANDSCAPING EDGING	50 SF	
B2	16+18	21.7 LT	LANDSCAPING BED	22 SF	
B2	16+68	28.0 RT	LANDSCAPING ROCKS	75 SF	
B2	16+81	20.5 LT	LANDSCAPING ROCKS	127 SF	
B2	17+22	20.5 LT	TIMBER EDGING	5 LF	
B2	17+61	20.7 LT	TIMBER EDGING	5 LF	
B2	17+95	50.0 RT	STAIRS	18 SF	
B3	18+03	25.0 LT	PAVERS	24 SF	
B3	18+24	47.6 RT	STAIRS	24 SF	
B3	18+27	32.5 RT	LANDSCAPING EDGING	54 SF	
B3	18+29	25.0 LT	PAVERS	30 SF	
B3	18+48	40.8 RT	RETAINING WALL (TIMBER)	42 LF	
B3	18+77	23.2 LT	LANDSCAPING ROCKS	23 SF	
B3	19+12	24.4 LT	LANDSCAPING ROCKS	12 SF	
B4	41+33	21.5 LT	LANDSCAPING ROCKS	46 SF	
B4	42+20	69.6 LT	RETAINING WALL (TIMBER)	47 LF	

70.23

REMOVE AND RELOCATE SHED (PARCEL 120) (12)

SHEET	STATION	OFFSET	LENGTH (FT)	WIDTH (FT)	AREA (SQ FT)	REMARKS
B4	40+24	145.2 LT	10.0	10.0	100.0	

75.11

SALVAGE AND RELOCATE OR DISPOSE EXISTING BOULDER	(13)
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SHEET	APPX STATION	APPX OFFSET (FT)	REMARKS
B1	12+29	26.0 RT	
B2	16+29	24.8 RT	
B2	16+78	26.4 RT	
B2	17+05	23.8 RT	
B2	17+17	23.8 RT	
B2	17+44	23.8 RT	
B4	43+51	28.2 RT	
B4	43+52	25.6 RT	
B4	43+52	30.3 RT	

File: J:\JobsData\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\01 Civil\Phase 1\10133.00 Demolition Plan - Phase 1.dwg

RECORD DRAWING		
1. DATA PROVIDED BY: _____	TITLE: _____	
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BY: _____	TITLE: _____	DATE: _____
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DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK
PLAN CHECK		

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
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PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50		IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	
ALL			
DEMOLITION SUMMARY			
SCALE		GRID SW1638, SW1738	
HOR. N/A VER. N/A		DATE JAN 2020	STATUS 95%
		SHEET	B7 of B8

14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	ALL				
DEMOLITION SUMMARY						
SCALE	<div style="display: flex; justify-content: space-between;"> <div style="width: 45%;"> HOR. N/A VER. N/A </div> <div style="width: 50%;"> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td colspan="2" style="padding: 2px;">GRID SW1635, SW1735</td> </tr> <tr> <td style="width: 50%; padding: 2px;">DATE JAN 2020</td> <td style="width: 50%; padding: 2px;">STATUS 95%</td> </tr> </table> </div> </div>	GRID SW1635, SW1735		DATE JAN 2020	STATUS 95%	B7 of B8
GRID SW1635, SW1735						
DATE JAN 2020	STATUS 95%					

B7 of B8

75.12

TEMPORARY TREE PROTECTION FENCE

SHEET	PROPOSED LOCATION				LENGTH (FT)	REMARKS
	APPROX BEGIN	APPROX BEGIN	APPROX END	APPROX END		
	STATION	OFFSET (FT)	STATION	OFFSET (FT)		
B1	9+24.8	129.2 LT	9+32.8	117.8 LT	16.6	
B1	9+32.8	117.8 LT	9+26.7	99.0 LT	20.8	
B1	9+26.7	99.0 LT	9+28.8	79.1 LT	20.1	
B1	9+28.8	79.1 LT	9+38.4	57.3 LT	25.0	
B1	9+38.4	57.3 LT	9+48.1	47.9 LT	15.0	
B1	9+48.1	47.9 LT	9+65.1	47.9 LT	20.2	
B1	9+65.1	47.9 LT	9+76.6	24.9 LT	26.4	
B1	9+84.3	73.9 LT	10+22.5	26.0 LT	66.2	
B1	13+51.8	31.2 RT	13+56.6	31.2 RT	4.6	
B1	13+56.6	31.2 RT	13+56.7	35.0 RT	3.8	
B1	40+37.8	27.6 LT	40+45.2	27.7 LT	7.4	
B1	40+45.2	27.7 LT	40+49.3	34.9 LT	8.3	
B2	15+10.5	36.3 RT	15+11.4	31.7 RT	4.7	
B2	15+54.3	29.0 RT	15+59.5	29.0 RT	5.0	
B2	15+59.5	29.0 RT	15+59.2	34.0 RT	5.0	
B2	16+16.7	24.1 LT	16+16.8	21.1 LT	3.0	
B2	16+16.8	21.1 LT	16+19.7	21.0 LT	3.1	
B2	16+19.7	21.0 LT	16+19.9	24.3 LT	3.3	
B2	16+77.1	28.3 RT	16+80.1	28.1 RT	2.8	
B2	16+80.1	28.1 RT	16+80.5	32.3 RT	4.3	
B2	17+06.0	27.5 RT	17+12.5	27.4 RT	6.2	
B2	17+44.4	26.3 RT	17+50.9	26.4 RT	6.4	
B3	19+77.0	30.4 RT	19+84.3	26.3 RT	9.6	
B3	19+84.3	26.3 RT	19+96.9	26.0 RT	14.8	
B4	41+35.2	170.0 LT	41+56.8	173.7 LT	21.9	
B4	41+56.8	173.7 LT	41+55.4	182.7 LT	9.1	
B4	41+55.4	182.7 LT	41+75.0	184.4 LT	19.8	
B4	41+75.0	184.4 LT	41+82.6	177.3 LT	10.4	
B4	41+85.2	49.3 LT	41+85.2	56.3 LT	7.0	
B4	41+85.2	56.3 LT	41+90.2	56.3 LT	5.0	
B4	41+90.2	56.3 LT	41+90.2	49.3 LT	7.0	
B4	41+90.2	49.3 LT	41+85.2	49.3 LT	5.0	
B4	42+18.2	171.8 LT	42+18.7	155.4 LT	16.4	

80.08

REMOVE JUNCTION BOX 14

SHEET	APPX STATION	APPX OFFSET (FT)	REMARKS
B1	9+97.4	18.2 LT	
B1	11+38.8	22.1 LT	
B2	14+46.2	20.5 LT	
B2	15+80.0	19.5 LT	
B3	19+74.5	19.0 LT	
B4	41+78.9	18.8 LT	
B4	43+47.8	21.1 LT	

80.28

REMOVE LUMINAIRE POLE (15)

SHEET	APPX STATION	APPX OFFSET (FT)	REMARKS
B1	9+96.8	21.9 LT	
B1	11+39.1	24.3 LT	
B1	12+99.9	23.6 LT	
B2	14+45.8	22.5 LT	
B2	15+80.5	21.6 LT	
B2	17+46.6	21.3 LT	FOUNDATION ONLY, SEE NOTE BELOW
B3	19+75.2	21.9 LT	
B4	41+79.3	21.6 LT	
B4	43+48.3	24.2 LT	

NOTE: CONTRACTOR SHALL REMOVE EXISTING ABANDONED LIGHT POLE FOUNDATION WITHIN PARCEL 138 DRIVEWAY. THE LOCATION OF THE ABANDONED LIGHT POLE SHOWN ON THE DRAWINGS IS APPROXIMATE. CONTRACTOR SHALL EXCAVATE DRIVEWAY AS REQUIRED TO LOCATE AND COMPLETELY REMOVE LIGHT POLE FOUNDATION. ALL WORK ASSOCIATED WITH LOCATING AND REMOVING EXISTING ABANDONED LIGHT POLE FOUNDATION SHALL BE INCLUDED IN THE 1 EACH 80.28 REMOVE LUMINAIRE POLE BID ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.

80.28

REMOVE LOAD CENTER (16)

SHEET	APPX STATION	APPX OFFSET (FT)	REMARKS
B1	40+36.7	165.7 LT	HEAT TRACE
B4	41+91.3	46.1 RT	ILLUMINATION
B4	42+14.5	35.5 LT	HEAT TRACE

RECORD DRAWING

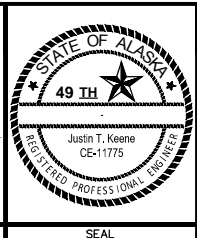
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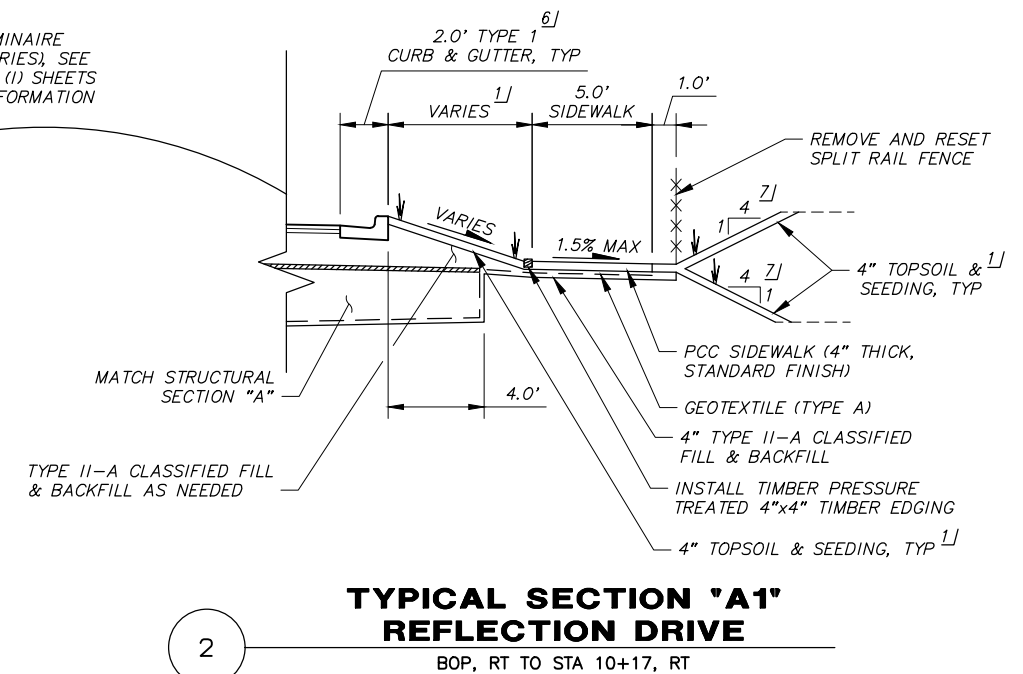
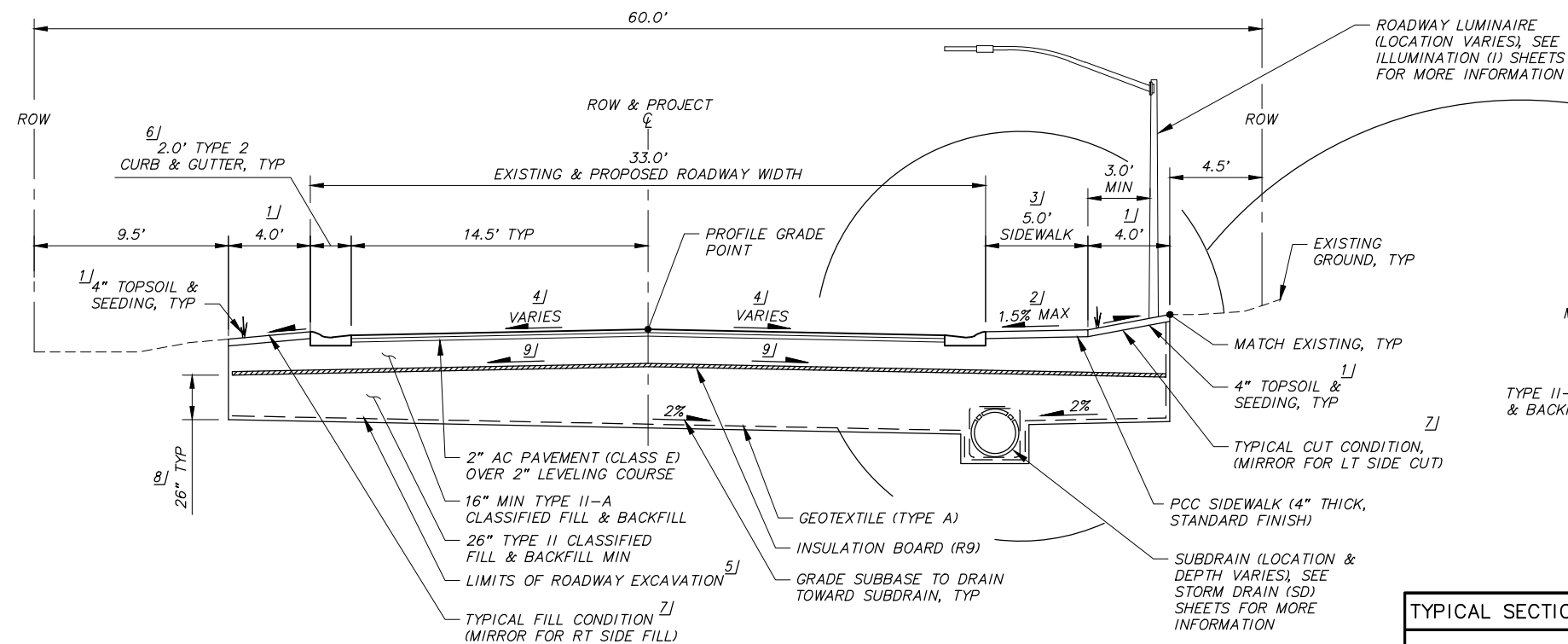


PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	ALL
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DEMOLITION SUMMARY

SCALE HOR. N/A VER. N/A GRID SW1638, SW1738 DATE JAN 2020 STATUS 95% SHEET B8 of B8



TYPICAL SECTION "A" CROSS SLOPE SUMMARY				
FROM STA	TO STA	CROSS SLOPE LEFT	CROSS SLOPE RIGHT	REMARKS
BOP	10+75	VARIES	1.25%	CROSS SLOPE LT VARIES FROM TIPPED ROADWAY TO CROWNED ROADWAY, SEE INTERSECTION LAYOUT SHEET R6
10+75	11+53	2.00%	1.25%	
11+53	11+81	VARIES	1.25%	CROSS SLOPE LT TRANSITION FROM 2.0% TO 2.5%, SEE INTERSECTION LAYOUT SHEET R6
11+81	19+32	2.50%	1.25%	
19+32	19+50	VARIES	VARIES	CROSS SLOPE LT TRANSITION FROM 2.5% TO 2.0%, CROSS SLOPE RT TRANSITION FROM 1.25% TO 2.0% SEE INTERSECTION LAYOUT SHEET R7
19+50	EOP	2.00%	2.00%	

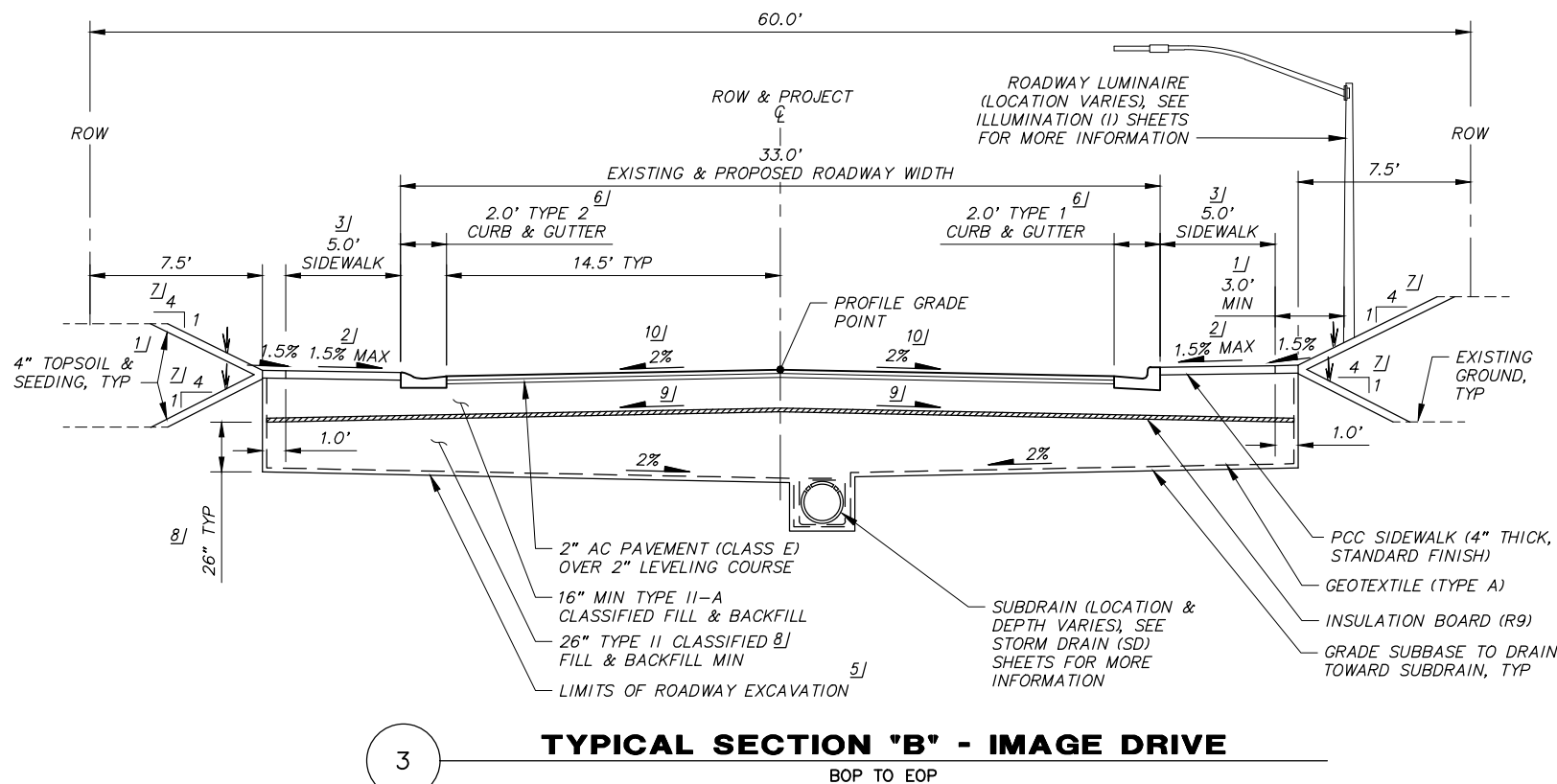
SEE SHEET NOTE 1.




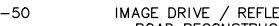
SHEET NOTES:

1. THE STATION RANGES ARE APPROXIMATE AND MAY BE MODIFIED IN THE FIELD BY THE ENGINEER.

#/ FOOT NOTES:

1. PLACE 4" OF TOPSOIL AND SEEDING (SCHEDULE A) ON ALL DISTURBED AREAS.
2. THE MAXIMUM SIDEWALK CROSS SLOPE GRADE IS 2% AT DRIVEWAYS.
3. INCREASE SIDEWALK THICKNESS TO 6" ACROSS ALL DRIVEWAYS & ADD WELDED STEEL WIRE REINFORCEMENT PER THE SPECIFICATIONS.
4. FOR PROPOSED CROSS SLOPES, SEE TYPICAL CROSS SLOPE SUMMARY TABLE, THIS SHEET.
5. PRIOR TO PLACEMENT OF FILL, NATIVE MATERIAL SHALL BE SCARIFIED, PROOF-ROLLED AND COMPACTED AS DIRECTED BY ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
6. TOP AC PAVEMENT SHALL BE 1/8" - 1/4" ABOVE LIP OF CURB, UNLESS OTHERWISE NOTED. SEE DETAIL 5, SHEET C3.
7. THE TYPICAL CUT/FILL SLOPES ARE 4 (HORIZONTAL): 1 (VERTICAL). MODIFY TYPICAL CUT/FILL SLOPES TO A MAXIMUM (STEEPEST) CUT/FILL SLOPE OF 2 (HORIZONTAL): 1 (VERTICAL) AS REQUIRED TO MATCH EXISTING GROUND WITHIN ROW. FILL SLOPES MAY VARY ALONG ROADWAY TO PROVIDE POSITIVE DRAINAGE TOWARD ROADWAY. SEE DETAIL 2, SHEET C3. SEE ROADWAY SHEETS FOR LOCATIONS. THE ENGINEER MAY ADJUST THE TYPICAL SLOPES IN THE FIELD.
8. THE MINIMUM 26" DEPTH OF TYPE II CLASSIFIED FILL & BACKFILL MATERIAL IS MEASURED AT THE EDGE OF EXCAVATION.
9. INSULATION SLOPE SHALL MATCH ROADWAY CROSS-SLOPE, SEE TYPICAL CROSS SLOPE SUMMARY TABLE, THIS SHEET.
10. ROADWAY CROSS SLOPE VARIES AT IMAGE DRIVE LOW POINT, SEE INTERSECTION LAYOUT SHEET R8.






RECORD DRAWING 1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ BY: _____ TITLE: _____ DATE: _____ 2. DATA TRANSFERRED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ 3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ BY: _____										<table border="1"> <tr> <th>DATA</th> <th>DRAWN BY</th> <th>CHECKED BY</th> </tr> <tr> <td>BASE</td> <td>TS</td> <td>MJ</td> </tr> <tr> <td>TOPOGRAPHY</td> <td>TS</td> <td>MJ</td> </tr> <tr> <td>PROFILE</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>STORM SEWER</td> <td>MV</td> <td>JH</td> </tr> <tr> <td>WATER/SANITARY SEWER</td> <td>MV</td> <td>JK</td> </tr> <tr> <td>GAS</td> <td>MV</td> <td>JK</td> </tr> <tr> <td>TELEPHONE</td> <td>MV</td> <td>JK</td> </tr> <tr> <td>ELECTRIC</td> <td>JH</td> <td>TK</td> </tr> <tr> <td>DESIGN</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>QUANTITIES</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>PRELIMINARY/FINAL</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>MUNICIPAL/STATE</td> <td>RB</td> <td>JK</td> </tr> </table>										DATA	DRAWN BY	CHECKED BY	BASE	TS	MJ	TOPOGRAPHY	TS	MJ	PROFILE	RB	JK	STORM SEWER	MV	JH	WATER/SANITARY SEWER	MV	JK	GAS	MV	JK	TELEPHONE	MV	JK	ELECTRIC	JH	TK	DESIGN	RB	JK	QUANTITIES	RB	JK	PRELIMINARY/FINAL	RB	JK	MUNICIPAL/STATE	RB	JK	<table border="1"> <tr> <th colspan="2">FIELD BOOKS</th> <th>BM NO.</th> <th>LOCATION</th> <th>ELEV.</th> <th>REV</th> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> </tr> <tr> <td>DESIGN CRW Books 147, 148, & 151</td> <td>GAAB 66</td> <td>See page D-24 of the MOA Benchmark Book</td> <td>238.10</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td></td> <td>GAAB 96</td> <td>See page D-26 of the MOA Benchmark Book</td> <td>313.83</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>STAKING</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>ASBUILT</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>CONTRACTOR</td> <td colspan="8">BASIS OF THIS DATUM GAAB 1972 ADJUST</td> </tr> <tr> <td>INSPECTOR</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> </table>										FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10							GAAB 96	See page D-26 of the MOA Benchmark Book	313.83						STAKING									ASBUILT									CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST								INSPECTOR																																				<div style="display: flex; justify-content: space-between;"> <div style="width: 45%;">  <p>CRW ENGINEERING GROUP LLC</p> <p>3940 ARCTIC BLVD, SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AECCL82-AK</p> </div> <div style="width: 45%; text-align: center;">  <p>Justin T. Keene CE-11775 REGISTERED PROFESSIONAL ENGINEER</p> </div> </div>										<div style="display: flex; justify-content: space-between;"> <div style="width: 45%;">  </div> <div style="width: 45%; text-align: center;">  </div> </div>									
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1. SEE SHEET C1 FOR ADJOINING ROADWAY SECTION.

1. PLACE 4" OF TOPSOIL AND SEEDING (SCHEDULE A) ON ALL DISTURBED AREAS.
2. THE MAXIMUM SIDEWALK CROSS SLOPE GRADE IS 2% AT DRIVEWAYS.
3. INCREASE SIDEWALK THICKNESS TO 6" ACROSS ALL DRIVEWAYS & ADD WELDED STEEL WIRE REINFORCEMENT PER THE SPECIFICATIONS.
4. ROADWAY CROSS SLOPE VARIES AT SOME LOCATIONS ALONG SIDE STREETS. SEE INTERSECTION LAYOUT SHEETS FOR LOCATIONS. MODIFY ROADWAY CROSS SLOPE AS REQUIRED TO MATCH INTO EXISTING ROADWAY OR AS DIRECTED IN THE FIELD BY THE ENGINEER. PROVIDE SMOOTH TRANSITION TO MATCH EXISTING AND TO PROVIDE POSITIVE DRAINAGE TOWARD DRAINAGE STRUCTURES.
5. PRIOR TO PLACEMENT OF FILL, NATIVE MATERIAL SHALL BE SCARIFIED, PROOF-ROLLED AND COMPACTED AS DIRECTED BY ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
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8. THE MINIMUM 26" DEPTH OF TYPE II CLASSIFIED FILL & BACKFILL MATERIAL IS MEASURED AT THE EDGE OF EXCAVATION.
9. SEE ROADWAY PLAN & PROFILE SHEETS FOR SIDE STREET WIDTHS.
10. SEE RECONSTRUCT DRIVEWAY SUMMARY TABLE & RECONSTRUCTION OF DRIVEWAY DETAILS FOR DRIVEWAY RECONSTRUCTION INFORMATION.



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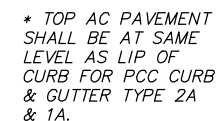
SPECIAL FILL GRADING DETAILS



SCALE: NTS



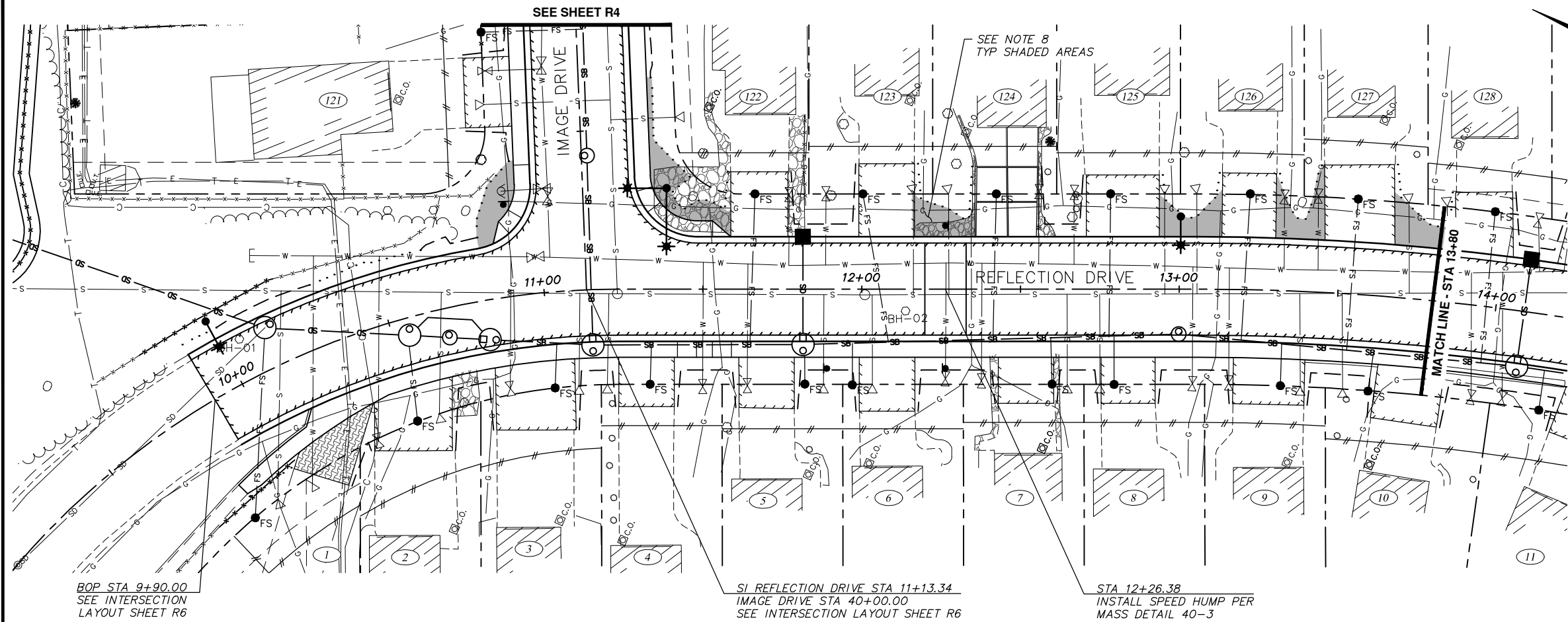
SCALE: NTS



CURB AND GUTTER & AC PAVEMENT EDGE DETAIL

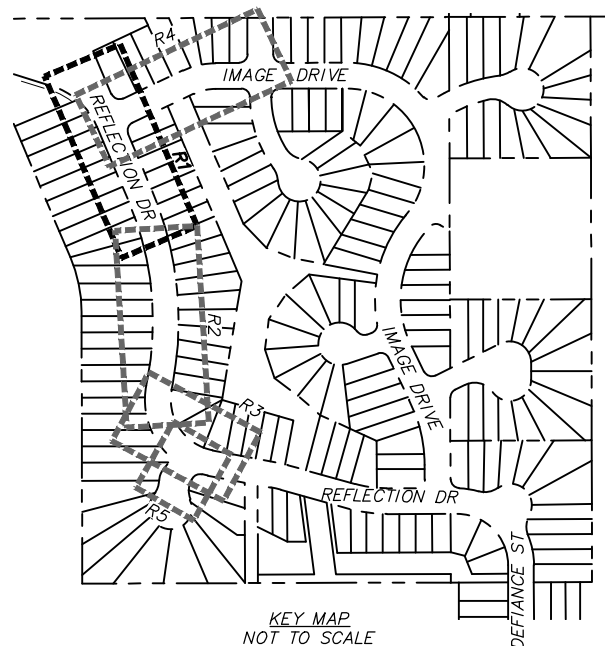
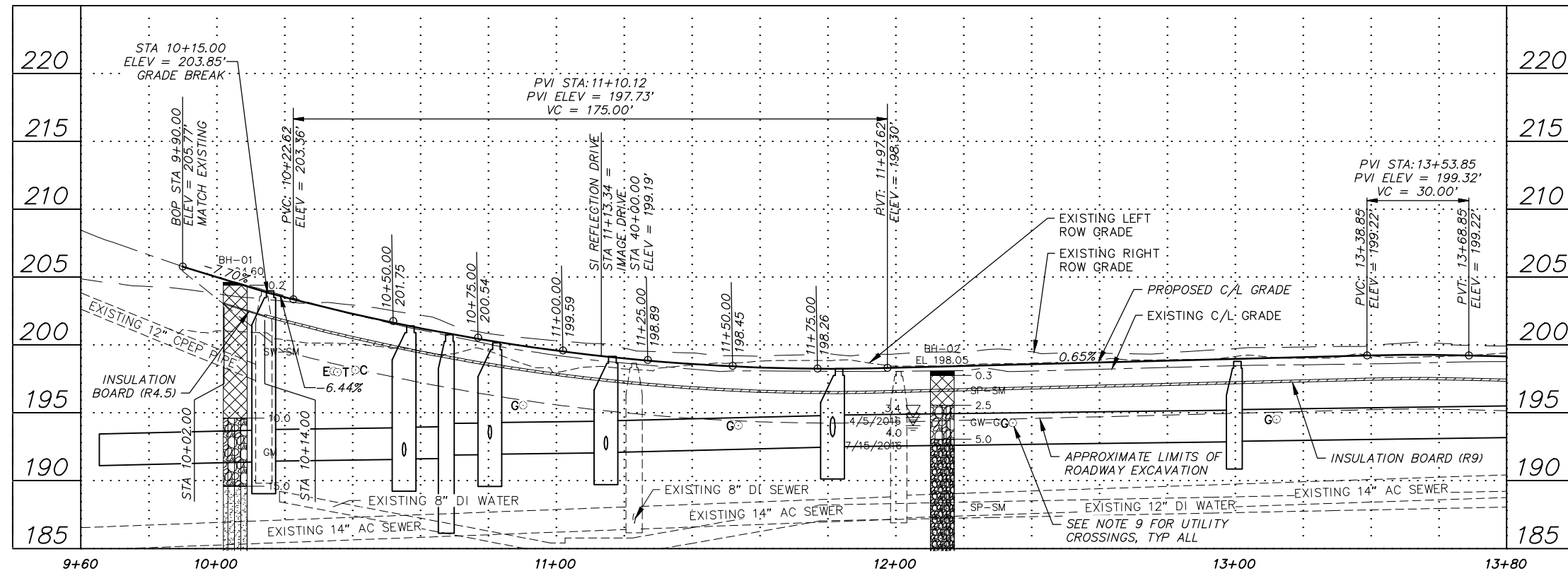
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File: s:\jobdata\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\01 Civil\Phase 1\10133.00 Plan & Profile - Roadway - Phase 1.dwg



NOTES:

- SEE ROADWAY SUMMARY TABLE (T) SHEETS FOR DETAILED ROADWAY INFORMATION.
- SEE DETAIL (D) SHEETS FOR ROADWAY DETAILS.
- FOR DETAILED SOILS INFORMATION, SEE THE SPECIFICATIONS.
- SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS AND ELEVATIONS OF STORM DRAIN PIPES & STRUCTURES.
- SEE SURVEY CONTROL (V) SHEETS FOR PROJECT CENTERLINE ALIGNMENT DATA.
- SEE ILLUMINATION (I) SHEETS FOR ROADWAY LIGHTING INFORMATION.
- THE DEMOLITION ITEMS REMOVED AS SHOWN ON THE DEMOLITION (B) SHEETS ARE NOT SHOWN FOR CLARITY.
- GRADE AREA TO DRAIN TOWARDS ROADWAY PER DETAIL 2, SHEET C3. NOTIFY ENGINEER IMMEDIATELY IF MIN 1.5% POSITIVE GRADE TOWARD ROADWAY CANNOT BE MAINTAINED. THIS WORK SHALL BE INCIDENTAL TO CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
- CAUTION!!! THE LOCATION OF EXISTING UTILITY CROSSINGS SHOWN IN PROFILE ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY LOCATION OF ALL UTILITIES ENCOUNTERED AND RECORD THEIR LOCATION ON THE CONTRACT RECORD DRAWINGS. DISCREPANCIES SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION TO THE ENGINEER. SEE SPECIFICATIONS FOR UTILITY RELOCATION SUMMARY.



RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____

BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

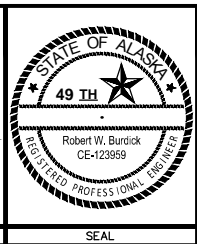
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COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

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INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							
REVISIONS							



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

14-50 IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1 SCHED A

ROADWAY PLAN & PROFILE

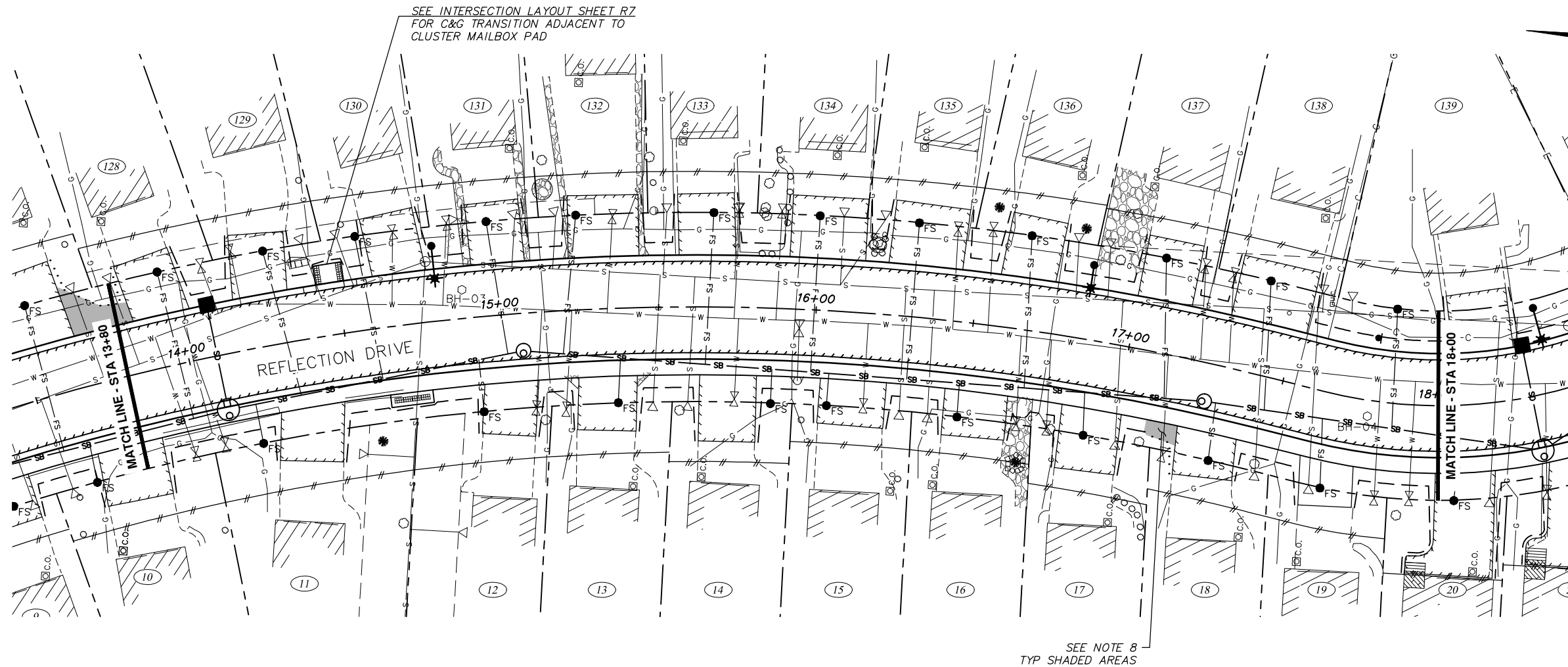
REFLECTION DRIVE
STA 9+90 TO 13+80

SCALE HOR. 1"=20'
VER. 1"=5'

GRID SW638, SW738

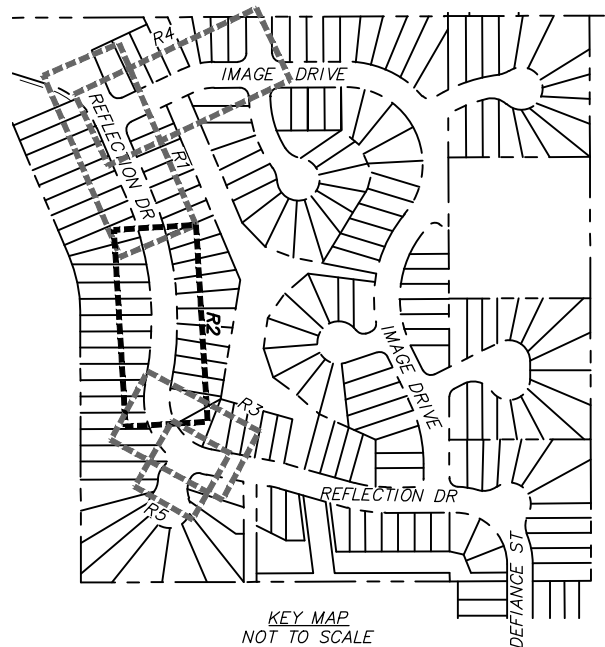
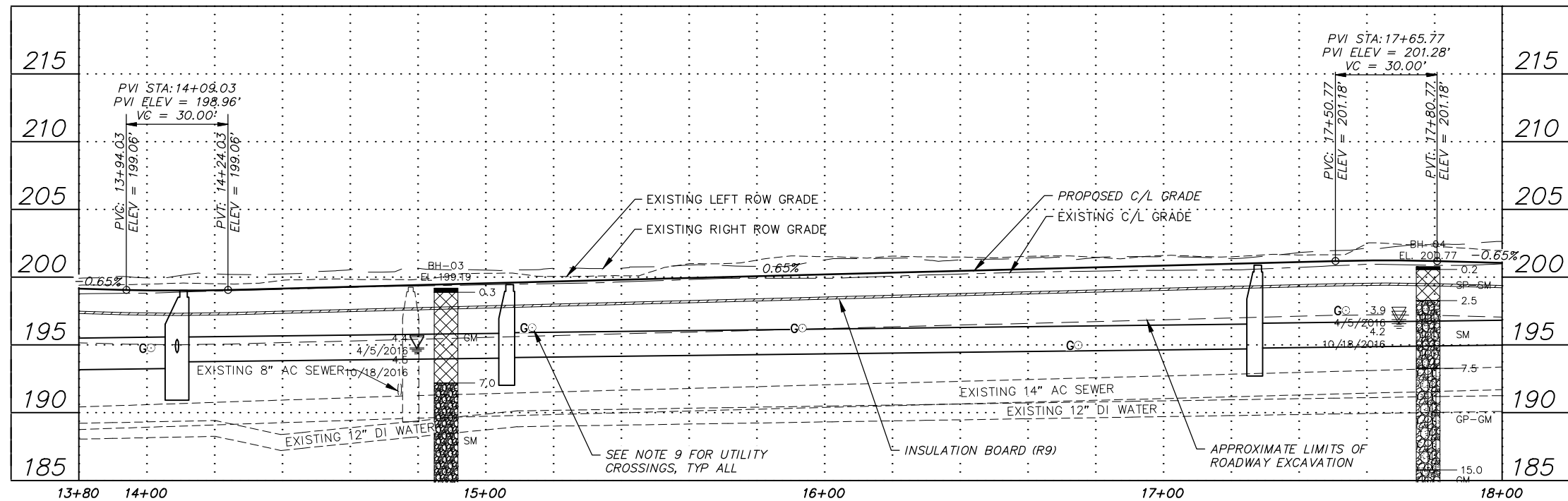
DATE JAN 2020 STATUS 95% SHEET R1 of R10

File: s:\jobdata\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\01 Civil\Phase 1\10133.00 Plan & Profile - Roadway - Phase 1.dwg



NOTES:

1. SEE ROADWAY SUMMARY TABLE (T) SHEETS FOR DETAILED ROADWAY INFORMATION.
2. SEE DETAIL (D) SHEETS FOR ROADWAY DETAILS.
3. FOR DETAILED SOILS INFORMATION, SEE THE SPECIFICATIONS.
4. SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS AND ELEVATIONS OF STORM DRAIN PIPES & STRUCTURES.
5. SEE SURVEY CONTROL (V) SHEETS FOR PROJECT CENTERLINE ALIGNMENT DATA.
6. SEE ILLUMINATION (I) SHEETS FOR ROADWAY LIGHTING INFORMATION.
7. THE DEMOLITION ITEMS REMOVED AS SHOWN ON THE DEMOLITION (B) SHEETS ARE NOT SHOWN FOR CLARITY.
8. GRADE AREA TO DRAIN TOWARDS ROADWAY PER DETAIL 2, SHEET C3. NOTIFY ENGINEER IMMEDIATELY IF MIN 1.5% POSITIVE GRADE TOWARD ROADWAY CANNOT BE MAINTAINED. THIS WORK SHALL BE INCIDENTAL TO CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
9. CAUTION!!! THE LOCATION OF EXISTING UTILITY CROSSINGS SHOWN IN PROFILE ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY LOCATION OF ALL UTILITIES ENCOUNTERED AND RECORD THEIR LOCATION ON THE CONTRACT RECORD DRAWINGS. DISCREPANCIES SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION TO THE ENGINEER. SEE SPECIFICATIONS FOR UTILITY RELOCATION SUMMARY.



RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____

BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

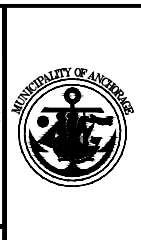
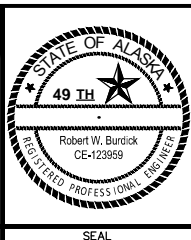
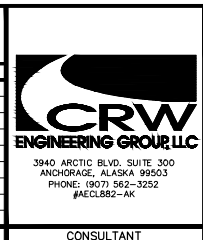
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COMPANY: _____ DATE: _____

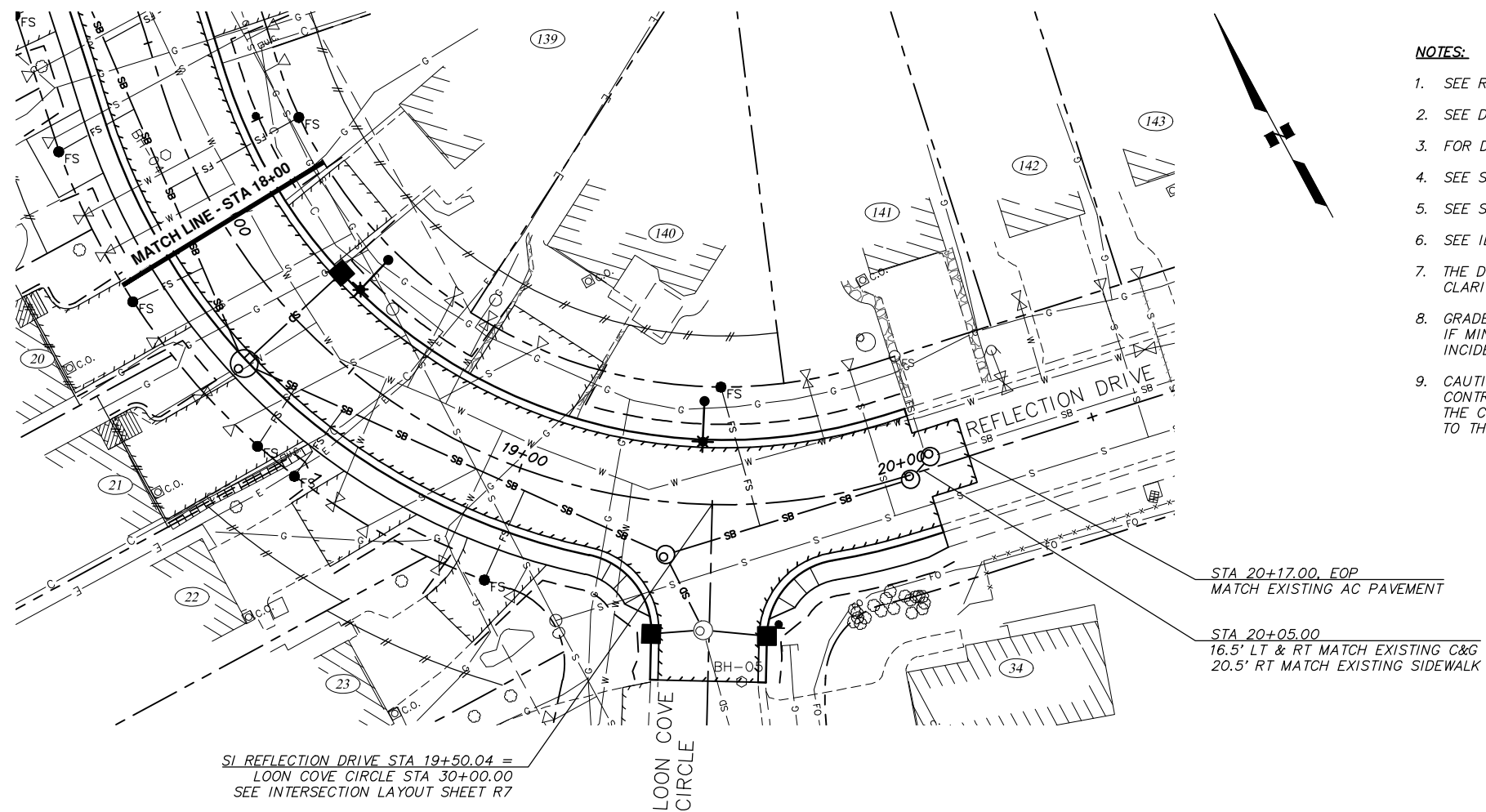
BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

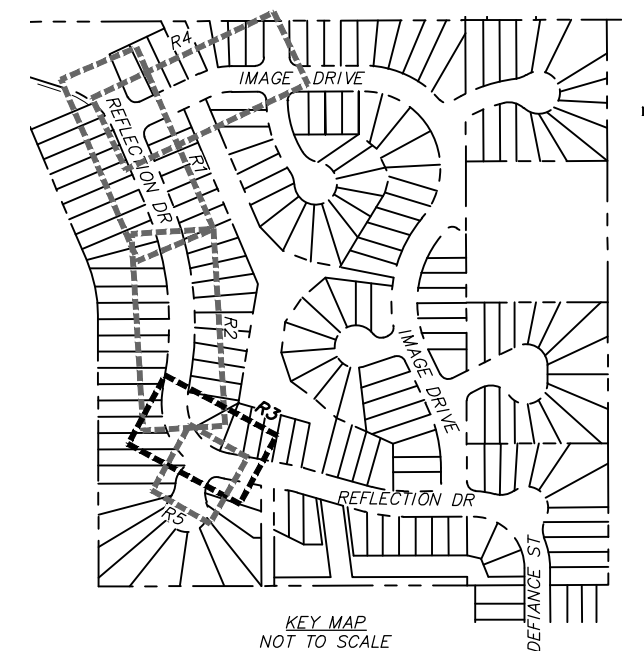
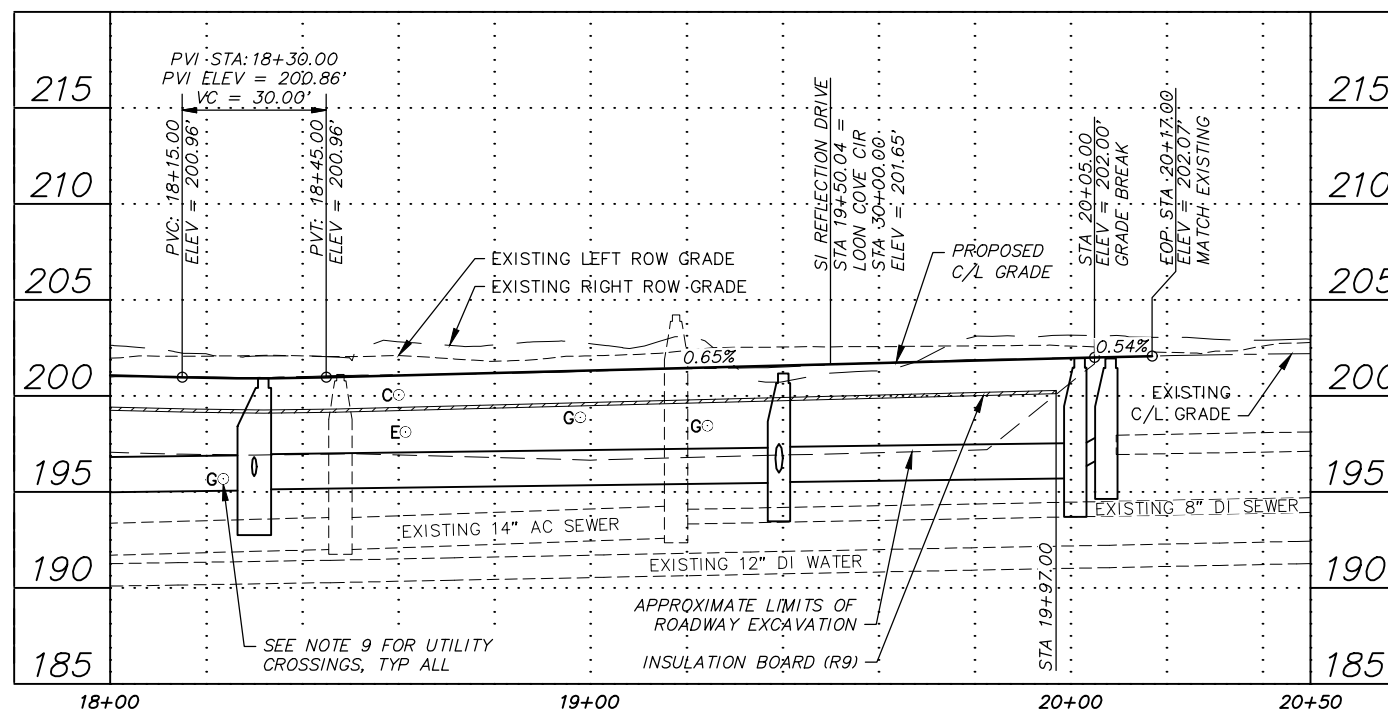
FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
	GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
STAKING							
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							
GRAPHIC SCALE 40 20 0 20 40							
PLAN CHECK							
CONSTRUCTION RECORD							
VERTICAL DATUM							
REVISIONS							
CONSULTANT							
SEAL							



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	SCHED A	
ROADWAY PLAN & PROFILE			
REFLECTION DRIVE STA 13+80 TO 18+00			
SCALE HOR. 1"=20' VER. 1"=5'	GRID SW1638, SW1738	DATE JAN 2020	STATUS 95%
SHEET			R2 of R10

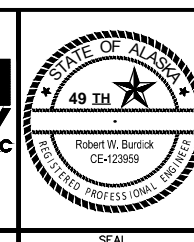


- NOTES:



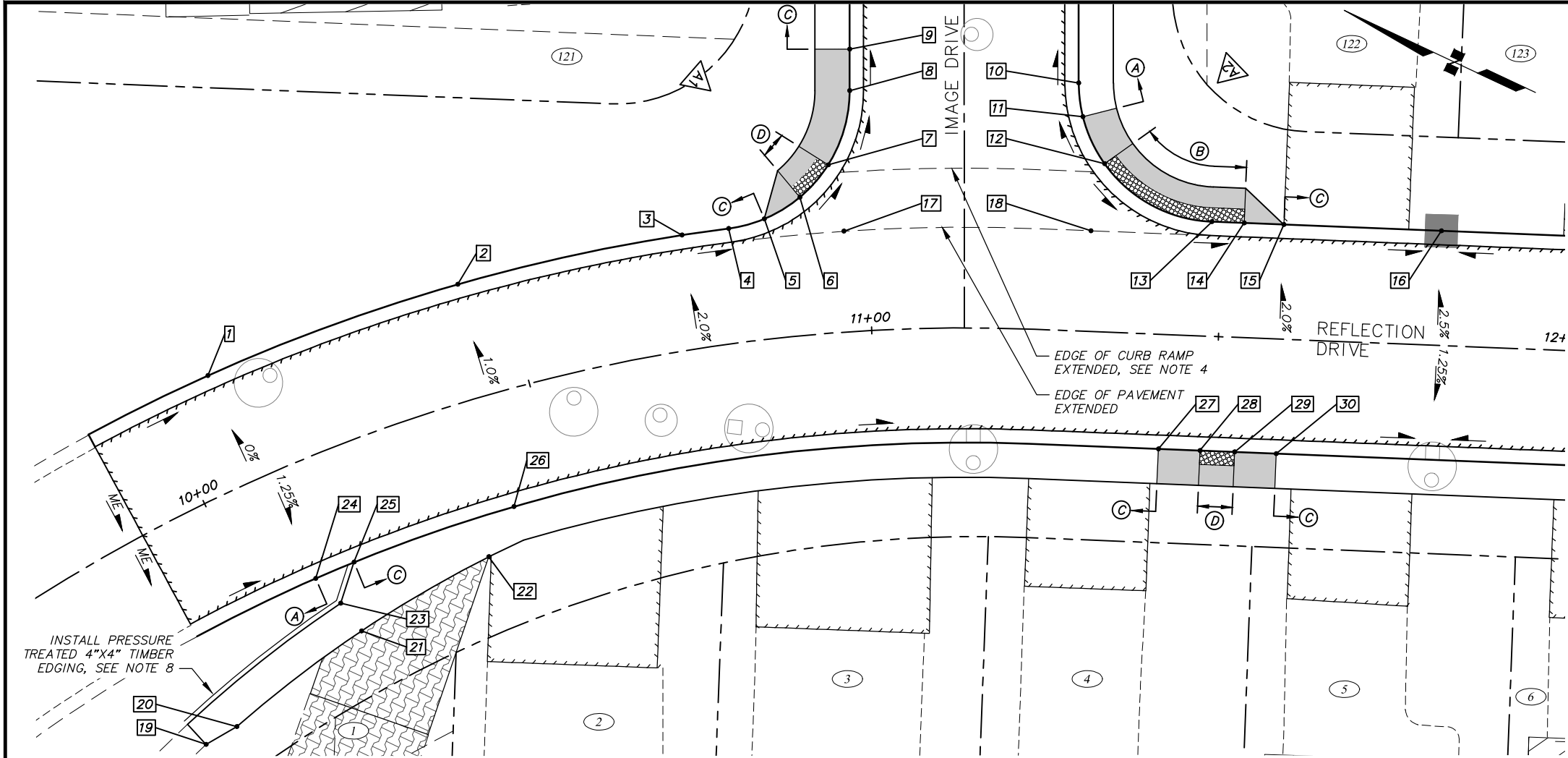
RECORD DRAWING		
1. DATA PROVIDED BY: _____	TITLE: _____	
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.		
CONTRACTOR: _____		
BY: _____	TITLE: _____	DATE: _____
2. DATA TRANSFERRED BY: _____	TITLE: _____	
COMPANY: _____	DATE: _____	
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.		
DATA TRANSFER CHECKED BY: _____	TITLE: _____	
COMPANY: _____	DATE: _____	
BY: _____		

DATA		DRAWN BY	CHECKED BY	<div><div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div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PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	SCHED A	
ROADWAY PLAN & PROFILE			
REFLECTION DRIVE STA 18+00 TO 20+17			
SCALE	HOR. 1"=20' VER. 1"=5'	GRID SW1638, SW1738 DATE JAN 2020 STATUS 95%	SHEET R3 of R10

File: s:\JobData\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\01 Civil\Phase 1\0133.00 Intersection Layout - Phase 1.dwg



NOTES

- SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
- SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.
- SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.
- THE MAXIMUM CROSS-SLOPE BETWEEN EDGE OF PAVEMENT EXTENDED AND EDGE OF CURB RAMP EXTENDED SHALL BE 2%. IF A 2% CROSS-SLOPE CANNOT BE MAINTAINED NOTIFY ENGINEER PRIOR TO INSTALLATION OF AC PAVEMENT.
- PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
- SEE DETAIL (D) SHEETS FOR CURB RAMP DETAILS.
- LIP OF CURB IS FRONT OF CURB AND GUTTER AT EDGE OF PAVEMENT.
- F&I PRESSURE TREATED TIMBER EDGING SHALL BE INCIDENTAL TO CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.

LEGEND

- APPROXIMATE DIRECTION OF DRAINAGE FLOWS
- DETECTABLE WARNING PANEL
- PCC CURB RAMP

DESIGNATION	CURB TYPE
(A)	TYPE 1 CURB
(B)	TYPE 1A CURB
(C)	TYPE 2 CURB
(D)	TYPE 2A CURB

CURB RADIUS TABLE				
POINT	TBC RADIUS POINT		RADIUS (FT)	DESCRIPTION
	STATION	OFFSET (FT)		
A1	10+81.41	36.5 LT	20.0	IMAGE DRIVE
A2	11+48.40	36.5 LT	20.0	IMAGE DRIVE

POINT SUMMARY – REFLECTION DRIVE AT IMAGE DRIVE

POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	CURB TYPE	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
							LENGTH (FT)	SLOPE (%)	
1	10+08.00	16.5 LT	204.56	2	204.39	—	38.09	−6.45%	BOC, SUPERELEVATION TRANSITION
2	10+44.00	16.5 LT	202.10	2	201.93	—	32.83	−5.11%	BOC, SUPERELEVATION TRANSITION
3	10+75.03	16.5 LT	200.42	2	200.25	—	6.75	−3.95%	BOC, SUPERELEVATION TRANSITION, NORMAL CROWN
4	10+81.41	16.5 LT	200.16	2	199.99	—	5.87	−3.98%	PC
5	10+86.35	17.3 LT	199.92	2	199.75	—	6.60	−3.98%	BEGIN FLARE
6	10+91.37	19.8 LT	199.52	2A	199.49	—	6.88	−1.80%	END FLARE, BEGIN LANDING
7	10+95.47	24.2 LT	199.40	2A	199.37	—	12.39	−6.91%	END LANDING, BEGIN RAMP
8	10+98.85	34.6 LT	198.66	N/A	198.51	—	5.99	−6.91%	PT
9	10+99.15	40.6 LT	198.26	2	198.09	—	—	—	END RAMP
10	11+28.41	35.7 LT	198.59	1	198.19	—	5.40	1.50%	PC
11	11+29.21	30.9 LT	198.67	1	198.27	—	8.25	1.50%	BEGIN RAMP
12	11+32.61	24.2 LT	198.38	1A	198.40	—	20.02	−1.10%	END RAMP, BEGIN LANDING
13	11+48.40	16.5 LT	198.16	1A	198.18	—	4.64	−1.21%	PT
14	11+53.04	16.5 LT	198.10	1A	198.12	—	5.74	−1.04%	END LANDING, BEGIN FLARE
15	11+58.77	16.5 LT	198.23	2	198.06	—	22.72	−0.78%	END FLARE
16	11+81.49	16.5 LT	198.05	2	197.88	—	—	—	CATCH BASIN

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

POINT SUMMARY – REFLECTION DRIVE AT IMAGE DRIVE

POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	CURB TYPE	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TOP OF CONCRETE ELEV (FT)	TO NEXT POINT*		DESCRIPTION
								LENGTH (FT)	SLOPE (%)	
17	10+96.94	14.5 LT	—	—	—	199.40	—	—	—	EDGE OF PAVEMENT EXTENDED
18	11+31.03	14.5 LT	—	—	—	198.47	—	—	—	EDGE OF PAVEMENT EXTENDED
19	9+82.90	30.7 RT	—	—	—	—	204.09	—	—	BACK OF SIDEWALK, BEGIN TRANSITION TO 5' SIDEWALK, MATCH EXISTING
20	9+88.75	30.7 RT	—	—	—	—	204.03	—	—	BACK OF SIDEWALK, END TRANSITION TO 5' SIDEWALK
21	10+13.75	26.1 RT	—	—	—	—	203.79	—	—	BACK OF SIDEWALK, GRADE BREAK
22	10+36.88	22.4 RT	—	—	—	—	202.56	—	—	BACK OF SIDEWALK, GRADE BREAK
23	10+12.41	21.2 RT	—	—	—	—	203.79	—	—	FRONT OF SIDEWALK
24	10+10.37	16.5 RT	204.42	1	204.02	—	—	6.05	−7.80%	BEGIN 1.25% RT CROSS SLOPE, BEGIN CURB TRANSITION
25	10+16.80	16.5 RT	203.72	2	203.55	—	—	24.63	−6.49%	END CURB TRANSITION
26	10+42.93	16.5 RT	202.12	2	201.95	—	—	—	—	BEGIN 5' SIDEWALK
27	11+41.98	16.5 RT	198.55	2	198.38	—	—	6.00	−1.48%	BEGIN RAMP
28	11+47.98	16.5 RT	198.32	2A	198.29	—	—	5.00	−1.26%	END RAMP, BEGIN LANDING
29	11+52.98	16.5 RT	198.26	2A	198.23	—	—	6.00	−1.03%	END LANDING, BEGIN RAMP
30	11+58.98	16.5 RT	198.34	2	198.17	—	—	—	—	END RAMP

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____ DATE: _____

BY: _____ TITLE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

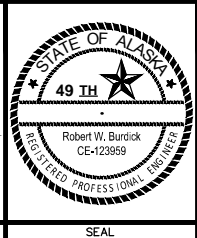
DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151		GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
		GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
STAKING								
ASBUILT								
CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM GAAB 1972 ADJUST								



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

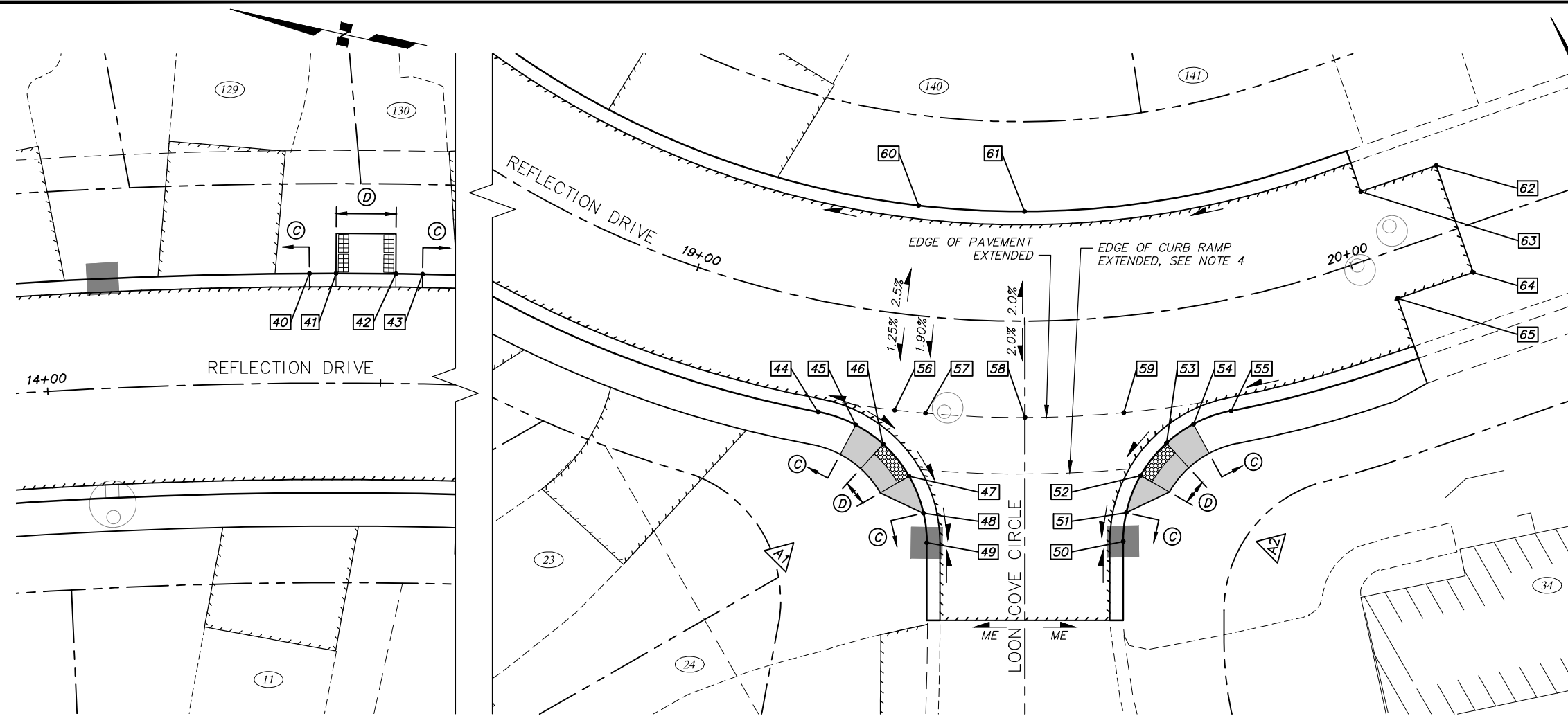
14-50 IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION – PHASE 1 SCHED A

INTERSECTION LAYOUT

IMAGE DRIVE

SCALE HOR. 1"=10' VER. N/A GRID SW1638, SW1738 DATE JAN 2020 STATUS 95% SHEET R6 of R10

File: I:\JobData\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\01 Civil\Phase 1\0133.00 Intersection Layout - Phase 1.dwg



NOTES

1. SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
2. SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.
3. SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.
4. THE MAXIMUM CROSS-SLOPE BETWEEN EDGE OF PAVEMENT EXTENDED AND EDGE OF CURB RAMP EXTENDED SHALL BE 2%. IF A 2% CROSS-SLOPE CANNOT BE MAINTAINED NOTIFY ENGINEER PRIOR TO INSTALLATION OF AC PAVEMENT.
5. PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
6. SEE DETAIL (D) SHEETS FOR CURB RAMP DETAILS.
7. LIP OF CURB IS FRONT OF CURB AND GUTTER AT EDGE OF PAVEMENT.

LEGEND

- APPROXIMATE DIRECTION OF DRAINAGE FLOWS
- ▨ DETECTABLE WARNING PANEL
- PCC CURB RAMP

DESIGNATION | CURB TYPE

- (C) TYPE 2 CURB
- (D) TYPE 2A CURB

△ CURB RADIUS TABLE

POINT	TBC RADIUS POINT		RADIUS (FT)	DESCRIPTION
	STATION	OFFSET (FT)		
A1	19+21.83	36.5 RT	20.0	LOON COVE CIRCLE
A2	19+78.07	36.5 RT	20.0	LOON COVE CIRCLE

□ POINT SUMMARY – REFLECTION DRIVE AT LOON COVE

POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	CURB TYPE	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
							LENGTH (FT)	SLOPE (%)	
40	14+39.41	16.5 LT	198.97	2	198.80	—	4.00	0.64%	BEGIN CURB TRANSITION
41	14+43.32	16.5 LT	198.85	2A	198.82	—	9.00	0.64%	END CURB TRANSITION, BEGIN LANDING
42	14+52.13	16.5 LT	198.91	2A	198.88	—	4.00	0.64%	END LANDING, BEGIN CURB TRANSITION
43	14+56.04	16.5 LT	199.08	2	198.91	—	—	—	END CURB TRANSITION
44	19+21.83	16.5 RT	201.45	2	201.28	—	6.60	0.70%	PC
45	19+27.13	17.5 RT	201.50	2	201.33	—	5.50	−0.95%	BEGIN RAMP
46	19+31.07	19.8 RT	201.30	2A	201.27	—	6.88	−1.80%	END RAMP, BEGIN LANDING
47	19+34.93	24.2 RT	201.18	2A	201.15	—	6.60	−2.72%	END LANDING, BEGIN FLARE
48	19+37.20	29.5 RT	201.14	2	200.97	—	4.91	−2.66%	END FLARE
49	19+37.90	33.9 RT	201.01	2	200.84	—	—	—	PT, CATCH BASIN
50	19+62.01	33.8 RT	201.28	2	201.11	—	4.80	2.62%	PC, CATCH BASIN
51	19+62.71	29.5 RT	201.40	2	201.23	—	6.60	2.62%	BEGIN FLARE
52	19+64.97	24.2 RT	201.44	2A	201.41	—	6.88	0.69%	END FLARE, BEGIN LANDING
53	19+68.83	19.8 RT	201.48	2A	201.45	—	5.50	0.69%	END LANDING, BEGIN RAMP
54	19+72.78	17.5 RT	201.66	2	201.49	—	6.60	0.69%	END RAMP
55	19+78.07	16.5 RT	201.71	2	201.54	—	—	—	PT

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

□ POINT SUMMARY – REFLECTION DRIVE AT LOON COVE

POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	CURB TYPE	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
							LENGTH (FT)	SLOPE (%)	
56	19+32.10	14.5 RT	—	—	—	201.35	—	—	EDGE OF PAVEMENT EXTENDED, BEGIN LANE SLOPE TRANSITION
57	19+36.34	14.5 RT	—	—	—	201.28	—	—	EDGE OF PAVEMENT EXTENDED
58	19+50.00	14.5 RT	—	—	—	201.35	—	—	EDGE OF PAVEMENT EXTENDED, END LANE SLOPE TRANSITION
59	19+63.56	14.5 RT	—	—	—	201.44	—	—	EDGE OF PAVEMENT EXTENDED
60	19+32.10	16.5 LT	201.34	—	201.34	—	16.14	1.17%	BEGIN LANE SLOPE TRANSITION
61	19+50.04	16.5 LT	201.53	—	201.53	—	—	—	END LANE SLOPE TRANSITION
62	20+17.00	10.0 LT	—	—	—	201.82	18.14	—	MATCH EXISTING
63	20+05.00	10.0 LT	—	—	—	201.75	19.14	—	MATCH EXISTING
64	20+17.00	7.0 RT	—	—	—	202.02	20.14	—	MATCH EXISTING
65	20+05.00	7.0 RT	—	—	—	201.96	—	—	MATCH EXISTING

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

RECORD DRAWING

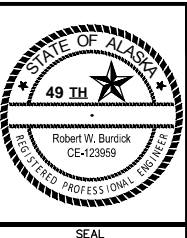
1. DATA PROVIDED BY: _____ TITLE: _____
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
CONTRACTOR: _____ DATE: _____
BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
DATA TRANSFER CHECKED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____
BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

GRAPHIC SCALE					
20	10	0	10	20	
FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV
DESIGN CRW Books 147, 148, & 151		GAAB 66	See page D-24 of the MOA Benchmark Book	238.10	
		GAAB 96	See page D-26 of the MOA Benchmark Book	313.83	
STAKING					
ASBUILT					
CONTRACTOR					
INSPECTOR					
BASIS OF THIS DATUM		GAAB 1972 ADJUST			
PLAN CHECK		CONSTRUCTION RECORD		VERTICAL DATUM	
		REVISIONS		CONSULTANT	



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION – PHASE 1	SCHED A
INTERSECTION LAYOUT		
REFLECTION DRIVE & CLUSTER MAILBOX		
SCALE HOR. 1"=10' VER. N/A	GRID SW1638, SW1738 DATE JAN 2020 STATUS 95%	SHEET R7 of R10

INSTALL TIMBER JOINTS WITHIN CONCRETE DRIVEWAY, SEE NOTE 6

123

124

125

MATCH EXISTING AT NEAREST JOINT

SEE NOTE 5

11' TYP.

10' TYP.

REFLECTION DRIVE

12+00

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____

BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____


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DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK
PLAN CHECK		

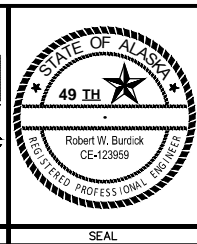
GRAPHIC				SCALE				
FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
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		GAAB 96	See page D-26 of the MOA Benchmark Book	313.63				
STAKING								
ASBUILT								
CONTRACTOR		BASIS OF THIS DATUM GAAB 1972 ADJUST						
INSPECTOR								
CONSTRUCTION RECORD		VERTICAL DATUM					REVISIONS	



CRW
ENGINEERING GROUP, LLC

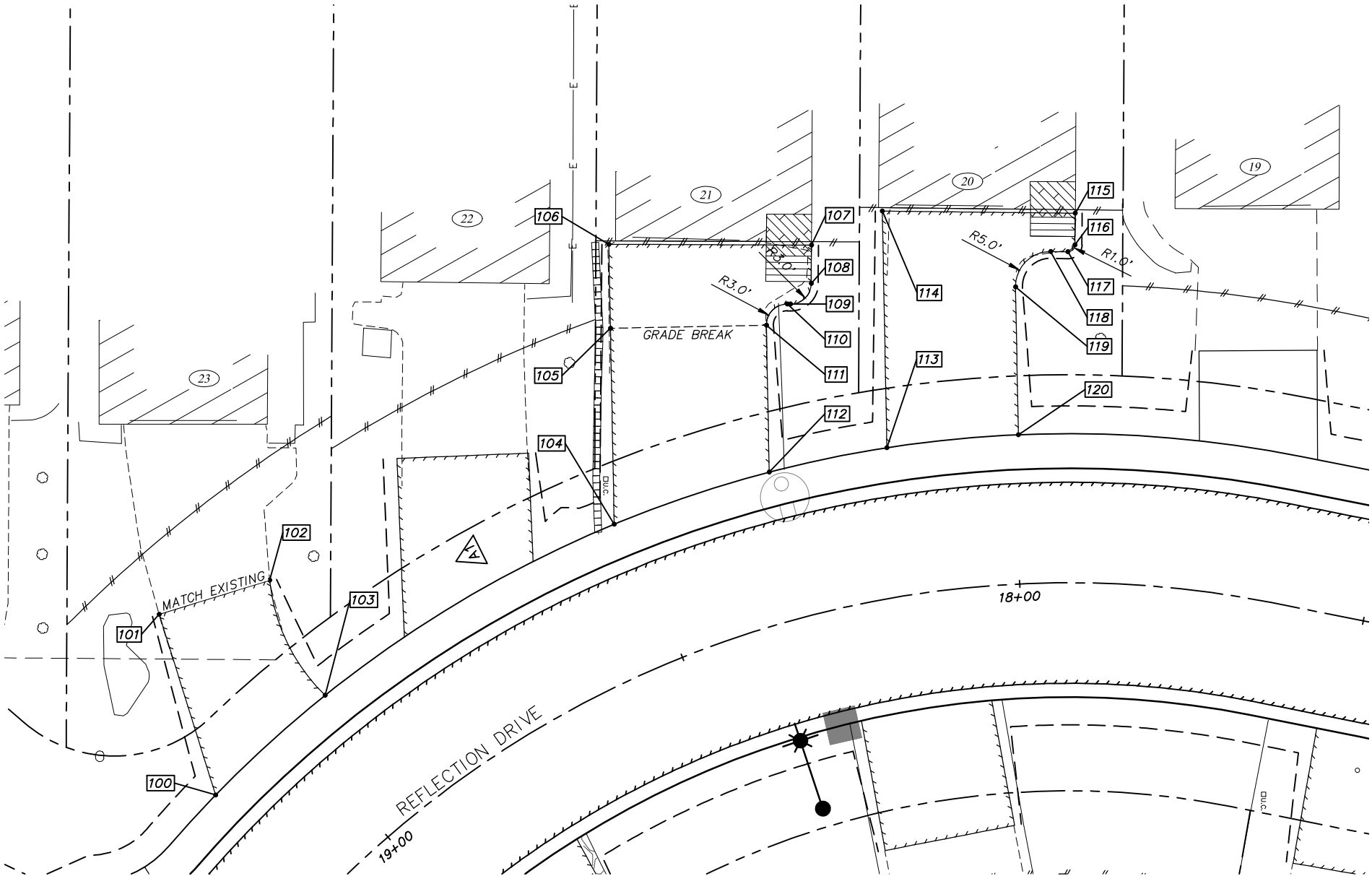
3940 ARCTIC BLVD, SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AEC1882-AK

CONSULTANT



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1		SCHED A
DRIVEWAY RECONSTRUCTION PLANS			
SCALE	HOR. 1"=10'	GRID SW1635, SW1735	R9 of R10
	VER. N/A	DATE JAN 2020	
SHEET			

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1 **PARCEL 20-23 DRIVEWAY RECONSTRUCTION PLAN**
SCALE: GRAPHIC

NOTES

1. SIDEWALK CROSS SLOPE SHALL BE 2% MAXIMUM AT DRIVEWAYS.
2. SEE 20.28 DRIVEWAY RECONSTRUCTION TABLE (T) SHEETS FOR DRIVEWAY INFORMATION.
3. SEE DRIVEWAY DETAILS (D) SHEETS FOR MORE INFORMATION.

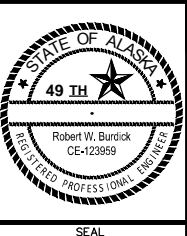
POINT SUMMARY – PARCELS 20–23				
POINT	STATION	OFFSET (FT)	ELEV (FT)	DESCRIPTION
100	19+12.97	21.5 RT	201.47	BACK OF SIDEWALK
101	19+02.98	45.9 RT	204.76	MATCH EXISTING
102	18+91.00	39.5 RT	204.28	MATCH EXISTING
103	18+94.29	21.5 RT	201.35	BACK OF SIDEWALK
104	18+51.76	21.5 RT	201.07	BACK OF SIDEWALK
105	18+43.95	48.1 RT	203.56	GRADE BREAK
106	18+41.21	59.6 RT	204.71	MATCH EXISTING
107	18+20.60	52.1 RT	204.32	MATCH EXISTING
108	18+21.39	46.7 RT	203.88	PC, R=3.0'
109	18+24.11	44.4 RT	203.51	PT
110	18+24.45	44.5 RT	203.47	PC, R=3.0'
111	18+27.25	42.2 RT	203.09	PT, GRADE BREAK
112	18+31.11	21.5 RT	200.98	BACK OF SIDEWALK
113	18+15.95	21.5 RT	201.02	BACK OF SIDEWALK
114	18+12.60	55.4 RT	205.12	MATCH EXISTING
115	17+92.21	53.3 RT	205.20	MATCH EXISTING
116	17+92.24	48.7 RT	204.71	PC, R=1.0'
117	17+93.00	47.7 RT	204.55	PT
118	17+94.89	47.8 RT	204.28	PC, R=5.0'
119	17+98.88	42.8 RT	203.42	PT
120	17+99.27	21.5 RT	201.13	BACK OF SIDEWALK

△ DRIVEWAY RADIUS TABLE				
POINT	TBC RADIUS POINT		RADIUS (FT)	DESCRIPTION
	STATION	OFFSET (FT)		
A1	18+71.46	26.8 RT	27.0	PARCEL 23

RECORD DRAWING
1. DATA PROVIDED BY: _____ TITLE: _____
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
CONTRACTOR: _____ DATE: _____
BY: _____
2. DATA TRANSFERRED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
DATA TRANSFER CHECKED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____
BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

<div>GRAPHIC</div> <div><div>20</div><div>10</div><div>0</div><div>10</div><div>20</div></div> <div>SCALE</div>								
FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151		GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
		GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
STAKING								
ASBUILT								
CONTRACTOR		BASIS OF THIS DATUM GAAB 1972 ADJUST						
INSPECTOR								
CONSTRUCTION RECORD		VERTICAL DATUM					REVISIONS	



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION – PHASE 1	SCHED A	
DRIVEWAY RECONSTRUCTION PLANS			
SCALE HOR. 1"=10' VER. N/A	GRID SW1638, SW1738 DATE JAN 2020	STATUS 95%	R10 of R10 SHEET

20.28

RECONSTRUCT DRIVEWAY

RECONSTRUCT DRIVEWAY NOTES:

SHEET	PARCEL	CENTERLINE REFERENCE		DRIVEWAY WIDTH AT CURB/ROW (FT)	SKEW ANGLE (DEGREES)	LANDING LENGTH (FT)	LANDING GRADE	TOTAL DISTANCE (FT)	EXISTING GRADE	PROPOSED GRADE	SURFACE TYPE ON PROPERTY	CONSTRUCT PER DETAIL	REMARKS
		STATION	OFFSET										
R1	1	10+29.67	RT	18	125	10.4	4.0%	27.6	2.8%	2.9%	CONCRETE	DETAIL 4, SHEET D2	SEE SHEET R9 FOR DRIVEWAY LAYOUT
R1	2	10+53.29	RT	24.5	105	5.1	3.0%	25.2	8.2%	7.7%	ASPHALT	DETAIL 4, SHEET D2	
R1	3	10+95.64	RT	25	96	5.0	1.9%	26.4	10.0%	8.8%	ASPHALT	DETAIL 4, SHEET D2	
R1	4	11+32.21	RT	17.5	90	5.0	1.5%	20.5	9.5%	8.7%	ASPHALT	DETAIL 4, SHEET D2	
R1	122	11+67.77	LT	18	-90	0.0	N/A	20.5	7.5%	5.6%	ASPHALT	DETAIL 5, SHEET D2	
R1	5	11+70.09	RT	17.5	90	5.0	2.0%	21.0	10.5%	10.5%	ASPHALT	DETAIL 4, SHEET D2	
R1	123	12+07.87	LT	17.5	-90	0.0	N/A	23.0	4.5%	3.0%	ASPHALT	DETAIL 5, SHEET D2	
R1	6	12+07.98	RT	17.5	90	5.0	1.5%	22.0	12.3%	11.4%	ASPHALT	DETAIL 4, SHEET D2	
R1	124	12+45.71	LT	20	-90	0.0	N/A	22.2	3.6%	2.4%	RED CONCRETE	DETAIL 5, SHEET D2	SEE SHEET R9 FOR DRIVEWAY LAYOUT
R1	7	12+51.75	RT	17.5	90	5.0	1.5%	19.5	10.8%	9.0%	ASPHALT	DETAIL 4, SHEET D2	
R1	125	12+82.23	LT	23	-90	0.0	N/A	19.5	4.5%	2.8%	ASPHALT	DETAIL 5, SHEET D2	
R1	8	12+83.94	RT	17	90	5.0	1.5%	19.5	10.6%	8.3%	ASPHALT	DETAIL 4, SHEET D2	
R1	126	13+21.60	LT	17	-90	0.0	2.0%	20.2	4.1%	2.0%	ASPHALT	DETAIL 5, SHEET D2	
R1	9	13+28.97	RT	22.5	90	5.0	1.5%	19.5	12.9%	9.6%	ASPHALT	DETAIL 4, SHEET D2	
R1	127	13+55.92	LT	22	-94	0.0	N/A	24.3	4.0%	1.5%	ASPHALT	DETAIL 5, SHEET D2	
R1	10	13+75.47	RT	21	93	5.0	1.5%	22.6	11.7%	10.0%	ASPHALT	DETAIL 4, SHEET D2	
						0.0							
R2	128	13+94.52	LT	18	-98	0.0	N/A	19.4	5.3%	4.4%	ASPHALT	DETAIL 5, SHEET D2	
R2	129	14+25.75	LT	17.5	-84	0.0	N/A	19.4	7.7%	5.5%	ASPHALT	DETAIL 5, SHEET D2	
R2	11	14+37.33	RT	20	101	5.1	1.4%	22.3	11.3%	11.3%	ASPHALT	DETAIL 4, SHEET D2	
R2	130	14+69.47	LT	17	-96	0.0	N/A	19.4	5.0%	6.0%	ASPHALT	DETAIL 5, SHEET D2	
R2	131	15+00.27	LT	17	-92	0.0	N/A	19.5	7.0%	7.3%	ASPHALT	DETAIL 5, SHEET D2	
R2	12	15+02.42	RT	17	98	5.0	1.9%	21.8	9.9%	10.0%	ASPHALT	DETAIL 4, SHEET D2	
R2	132	15+33.18	LT	22.5	-90	0.0	N/A	19.5	5.2%	4.2%	ASPHALT	DETAIL 5, SHEET D2	
R2	13	15+33.53	RT	18.5	93	5.0	1.5%	22.1	11.5%	10.3%	ASPHALT	DETAIL 4, SHEET D2	
R2	133	15+65.46	LT	18	-92	0.0	N/A	19.5	9.2%	9.2%	ASPHALT	DETAIL 5, SHEET D2	
R2	14	15+72.41	RT	18	90	5.0	1.5%	26.1	9.4%	9.5%	ASPHALT	DETAIL 4, SHEET D2	
R2	134	16+02.81	LT	24	-90	0.0	N/A	19.5	12.4%	11.2%	ASPHALT	DETAIL 5, SHEET D2	
R2	15	16+13.42	RT	21	90	5.0	1.5%	19.5	13.6%	12.6%	ASPHALT	DETAIL 4, SHEET D2	
R2	135	16+33.33	LT	23.5	-88	0.0	N/A	19.5	10.8%	9.7%	ASPHALT	DETAIL 5, SHEET D2	
R2	16	16+53.48	RT	20	88	5.0	1.5%	19.5	10.8%	10.2%	ASPHALT	DETAIL 4, SHEET D2	
R2	136	16+65.41	LT	17.5	-86	0.0	N/A	19.5	8.9%	8.6%	ASPHALT	DETAIL 5, SHEET D2	
R2	17	16+91.49	RT	19	87	5.0	1.5%	24.8	10.8%	9.4%	ASPHALT	DETAIL 4, SHEET D2	
R2	137	17+08.37	LT	17.5	-90	0.0	N/A	19.5	7.7%	7.0%	ASPHALT	DETAIL 5, SHEET D2	
R2	18	17+29.49	RT	19	84	5.0	1.6%	19.4	8.9%	7.2%	ASPHALT	DETAIL 4, SHEET D2	
R2	138	17+41.43	LT	24.5	-90	0.0	N/A	19.5	7.6%	6.2%	ASPHALT	DETAIL 5, SHEET D2	
R2	19	17+68.13	RT	17	79	5.1	1.5%	19.2	11.9%	11.0%	CONCRETE	DETAIL 4, SHEET D2	
R3	20	18+08.03	RT	VARIES	95	5.0	2.0%	37.9	13.6%	13.0%	ASPHALT	DETAIL 4, SHEET D2	SEE SHEET R10 FOR DRIVEWAY LAYOUT
R3	139	18+16.13	LT	19	-90	-16.5	N/A	19.5	9.2%	9.3%	ASPHALT	DETAIL 5, SHEET D2	
R3	21	18+43.21	RT	VARIES	110	5.3	1.8%	41.6	9.7%	VARIES	ASPHALT	DETAIL 4, SHEET D2	SEE SHEET R10 FOR DRIVEWAY LAYOUT
R3	22	18+74.69	RT	19	122	5.7	1.0%	26.0	14.4%	14.2%	ASPHALT	DETAIL 4, SHEET D2	SEE SHEET R10 FOR DRIVEWAY LAYOUT
R3	140	18+92.26	LT	27	-81	-16.7	N/A	19.3	6.6%	7.0%	ASPHALT	DETAIL 5, SHEET D2	
R3	23	19+06.94	RT	16.5	119	5.6	1.1%	28.6	13.6%	13.5%	ASPHALT	DETAIL 4, SHEET D2	SEE SHEET R10 FOR DRIVEWAY LAYOUT

1. "LANDING LENGTH" BEGINS AT THE BACK OF CURB & GUTTER.
2. "LANDING GRADE" IS THE GRADE OF THE LANDING FROM THE BACK OF CURB & GUTTER TO THE END OF LANDING.
3. "SKEW ANGLE" ("+" IS CLOCKWISE AND "-" IS COUNTER CLOCKWISE) IS MEASURED FROM PROJECT CENTERLINE WITH 0 DEGREES ALIGNED ALONG INCREASING STATIONS.
4. "TOTAL DISTANCE" IS THE LIMIT OF RECONSTRUCTION BEGINNING AT THE BACK OF CURB & GUTTER.
5. "PROPOSED GRADE" IS APPROXIMATE GRADE FROM THE END OF THE LANDING TO THE LIMIT OF RECONSTRUCTION. ACTUAL CONSTRUCTION GRADE MAY VARY.
6. WIDTHS, LENGTHS & GRADES PRESENTED IN THE DRIVEWAY SUMMARY TABLE ARE MEASURED ALONG SKEW ANGLE AND MAY NOT BE PERPENDICULAR TO ROADWAY CENTERLINE ALIGNMENT.
7. SEE SHEET T2 FOR RECONSTRUCT DRIVEWAY SUMMARY TABLE CONTINUED.

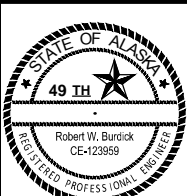
RECORD DRAWING

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DATA	DRAWN BY	CHECKED BY							
BASE	TS	MJ							
TOPOGRAPHY	TS	MJ							
PROFILE	RG	JK							
STORM SEWER	MV	JH							
WATER/SANITARY SEWER	MV	JK							
GAS	MV	JK							
TELEPHONE	MV	JK							
ELECTRIC	JH	TK							
DESIGN	RB	JK							
QUANTITIES	RB	JK							
PRELIMINARY/FINAL	RB	JK							
MUNICIPAL/STATE	RB	JK							
PLAN CHECK			CONSTRUCTION RECORD		VERTICAL DATUM		REVISIONS		



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	SCHED A
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ROADWAY SUMMARY TABLES

SCALE HOR. N/A
VER. N/A

GRID SW1638, SW1738	
DATE JAN 2020	STATUS 95%

SHEET 1 of 4

30.03

P.C.C. SIDEWALK

SHEET	APPX BEGIN STA	APPX OFFSET (FT)	APPX END STA	APPX OFFSET (FT)	4" THICK, AREA (SY)	6" THICK, AREA (SY)	REMARKS
R1	9+81.9	26.9 RT	10+06.9	22.2 RT	10	0	
R1	10+06.9	22.2 RT	10+65.6	16.5 RT	0	33	PARCEL 1-2 DRIVEWAY
R1	10+65.6	16.5 RT	10+80.7	16.5 RT	8	0	
R1	10+80.7	16.5 RT	11+08.3	16.5 RT	0	14	PARCEL 3 DRIVEWAY
R1	11+08.3	16.5 RT	11+23.5	16.5 RT	8	0	
R1	11+23.5	16.5 RT	11+42.0	16.5 RT	0	10	PARCEL 4 DRIVEWAY
R1	11+59.0	16.5 RT	11+78.8	16.5 RT	0	11	PARCEL 5 DRIVEWAY
R1	11+78.8	16.5 RT	11+99.2	16.5 RT	11	0	
R1	11+99.2	16.5 RT	12+16.7	16.5 RT	0	10	PARCEL 6 DRIVEWAY
R1	12+16.7	16.5 RT	12+43.0	16.5 RT	15	0	
R1	12+43.0	16.5 RT	12+60.5	16.5 RT	0	10	PARCEL 7 DRIVEWAY
R1	12+60.5	16.5 RT	12+75.4	16.5 RT	8	0	
R1	12+75.4	16.5 RT	12+92.4	16.5 RT	0	9	PARCEL 8 DRIVEWAY
R1	12+92.4	16.5 RT	13+17.4	16.5 RT	14	0	
R1	13+17.4	16.5 RT	13+40.6	16.5 RT	0	13	PARCEL 9 DRIVEWAY
R1	13+40.6	16.5 RT	13+64.4	16.5 RT	13	0	
R1	13+64.4	16.5 RT	13+86.0	16.5 RT	0	12	PARCEL 10 DRIVEWAY
R2	13+86.0	16.5 RT	14+25.8	16.5 RT	22	0	
R2	14+25.8	16.5 RT	14+46.8	16.5 RT	0	11	PARCEL 11 DRIVEWAY
R2	14+46.8	16.5 RT	14+92.9	16.5 RT	25	0	
R2	14+93.9	16.5 RT	15+10.6	16.5 RT	0	10	PARCEL 12 DRIVEWAY
R2	15+10.6	16.5 RT	15+23.7	16.5 RT	7	0	
R2	15+23.7	16.5 RT	15+42.8	16.5 RT	0	10	PARCEL 13 DRIVEWAY
R2	15+42.8	16.5 RT	15+63.1	16.5 RT	11	0	
R2	15+63.1	16.5 RT	15+81.6	16.5 RT	0	10	PARCEL 14 DRIVEWAY
R2	15+81.6	16.5 RT	16+02.5	16.5 RT	11	0	
R2	16+02.5	16.5 RT	16+24.2	16.5 RT	0	12	PARCEL 15 DRIVEWAY
R2	16+24.2	16.5 RT	16+43.4	16.5 RT	10	0	
R2	16+43.4	16.5 RT	16+64.0	16.5 RT	0	11	PARCEL 16 DRIVEWAY
R2	16+64.0	16.5 RT	16+82.0	16.5 RT	10	0	
R2	16+82.0	16.5 RT	17+01.6	16.5 RT	0	11	PARCEL 17 DRIVEWAY
R2	17+01.6	16.5 RT	17+20.2	16.5 RT	10	0	
R2	17+20.2	16.5 RT	17+39.9	16.5 RT	0	11	PARCEL 18 DRIVEWAY
R2	17+39.9	16.5 RT	17+61.0	16.5 RT	12	0	
R2	17+61.0	16.5 RT	17+76.4	16.5 RT	0	36	PARCEL 19 DRIVEWAY
R2	17+76.4	16.5 RT	17+99.1	16.5 RT	14	0	

30.12

REMOVE AND RESET INTERLOCKING CONCRETE PAVERS

SHEET	APPX BEGIN STA	APPX OFFSET (FT)	APPX END STA	APPX OFFSET (FT)	AREA (SF)	REMARKS
R1	10+06.3	27.4 RT	10+36.9	22.4 RT	301	PARCEL 1 DRIVEWAY — SEE SHEET R9

30.03

P.C.C. SIDEWALK (CONTINUED)

SHEET	APPX BEGIN STA	APPX OFFSET (FT)	APPX END STA	APPX OFFSET (FT)	4" THICK, AREA (SY)	6" THICK, AREA (SY)	REMARKS
R3	17+99.1	16.5 RT	18+16.0	16.5 RT	0	11	PARCEL 20 DRIVEWAY
R3	18+16.0	16.5 RT	18+31.1	16.5 RT	9	0	
R3	18+31.1	16.5 RT	18+51.8	16.5 RT	0	13	PARCEL 21 DRIVEWAY
R3	18+51.8	16.5 RT	18+63.0	16.5 RT	7	0	
R3	18+63.0	16.5 RT	18+81.9	16.5 RT	0	12	PARCEL 22 DRIVEWAY
R3	18+81.9	16.5 RT	18+94.3	16.5 RT	8	0	
R3	18+94.3	16.5 RT	19+13.0	16.5 RT	0	12	PARCEL 23 DRIVEWAY
R3	19+13.0	16.5 RT	19+27.1	17.5 RT	8	0	
R3	19+27.1	17.5 RT	19+42.2	17.5 RT	19	0	
R4	40+30.4	17.1 RT	43+47.3	17.2 RT	171	0	
R4	40+40.1	16.5 LT	40+74.5	16.5 LT	0	19	PARCEL 121 DRIVEWAY
R4	40+74.5	16.5 LT	41+02.9	16.5 LT	16	0	
R4	41+02.9	16.5 LT	41+30.9	16.5 LT	0	16	PARCEL 120 DRIVEWAY
R4	41+30.9	16.5 LT	41+48.2	16.5 LT	10	0	
R4	41+48.2	16.5 LT	41+67.2	16.5 LT	0	11	PARCEL 119 DRIVEWAY
R4	41+67.2	16.5 LT	42+25.8	16.5 LT	34	0	
R4	42+25.8	16.5 LT	42+40.3	16.5 LT	0	9	PARCEL 117 DRIVEWAY
R4	42+40.3	16.5 LT	42+50.7	16.5 LT	7	0	
R4	42+50.7	16.5 LT	42+64.2	16.5 LT	0	8	PARCEL 116 DRIVEWAY
R4	42+64.2	16.5 LT	42+77.6	16.5 LT	8	0	
R4	42+77.6	16.5 LT	42+96.6	16.5 LT	0	11	PARCEL 115 DRIVEWAY
R4	42+96.6	16.5 LT	43+13.0	16.5 LT	9	0	
R4	43+13.0	16.5 LT	43+29.0	16.5 LT	0	9	PARCEL 114 DRIVEWAY
R4	43+29.0	16.5 LT	43+48.9	18.3 LT	11	0	

30.10

COLORED CONCRETE (6" THICK, RED, BROOM FINISH)

SHEET	APPX BEGIN STA	APPX OFFSET (FT)	APPX END STA	APPX OFFSET (FT)	AREA (SY)	REMARKS
R1	12+35.7	16.5 LT	12+55.7	16.5 LT	50	PARCEL 124 DRIVEWAY – SEE SHEET R9

RECORD DRAWING

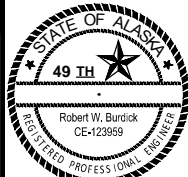
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TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
	GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
STAKING							
ASBUILT							
CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST						
INSPECTOR							
CONSTRUCTION RECORD		VERTICAL DATUM				REVISIONS	

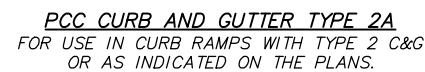


PROJECT MANAGEMENT AND ENGINEERING
DEPARTMENT

14-50 IMAGE DRIVE / REFLECTION DRIVE AREA SCHED A
ROAD RECONSTRUCTION - PHASE 1

ROADWAY SUMMARY TABLES

SCALE HOR. N/A VER. N/A GRID SW1638, SW1738 DATE JAN 2020 STATUS 95% SHEET T3 of T4



SCALE: NTS

ACCESSIBLE CURB & GUTTER NOTES:

1. TRANSITION CURBS TO MAINTAIN CONSTANT FLOWLINE ACROSS CURB RAMP AND AROUND CURB RETURN IAW PLANS.
2. PAYMENT FOR ALL PCC CURB AND GUTTER, INCLUDING MODIFIED AND TRANSITIONAL CURB, SHALL BE PAID UNDER THE BID ITEM "PCC CURB & GUTTER (ALL TYPES)" AND NO SEPARATE PAYMENT SHALL BE MADE.

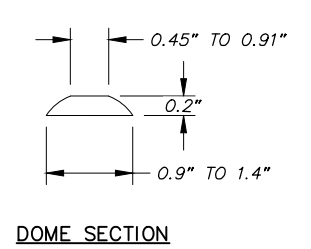
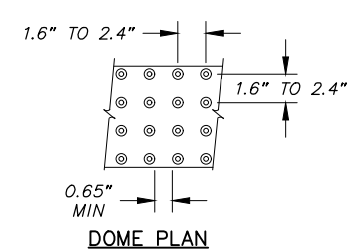
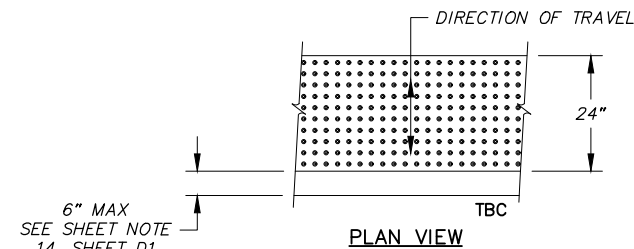
SHEET DRIVEWAY NOTES:

1. ALL SLOPES ARE IN REFERENCE TO THE HORIZONTAL.
2. PAYMENT FOR PCC CURB & GUTTER (ALL TYPES) AND TRANSITION C&G SHALL BE PAID UNDER THE BID ITEM "PCC CURB & GUTTER (ALL TYPES)" AND NO SEPARATE PAYMENT SHALL BE MADE.
3. CENTER THE PROPOSED DRIVEWAY ENTRANCES ON DRIVEWAY CENTERLINE REFERENCE POINT AS SHOWN IN THE 20.28 RECONSTRUCT DRIVEWAY SUMMARY TABLES PROVIDED ON THE ROADWAY SUMMARY TABLE "T" SHEETS.
4. INCREASE SIDEWALK THICKNESS TO 6" ACROSS ALL DRIVEWAYS AND IN BETWEEN DRIVEWAYS THAT ARE WITHIN 5 FEET OF ADJACENT DRIVEWAYS & INSTALL WELDED WIRE MESH PER THE SPECIFICATIONS.
5. SEE 20.28 DRIVEWAY RECONSTRUCTION SUMMARY TABLES ON THE ROADWAY SUMMARY TABLE "T" AND DRIVEWAY RECONSTRUCTION DETAILS, FOR INDIVIDUAL DRIVEWAY SPECIFICS.
6. THE MAXIMUM CROSS SLOPE AT DRIVEWAYS IS 2%.



DETECTABLE WARNING PANEL

SCALE: NTS





BACKING CURB DETAIL

SCALE: NTS

BACKING CURB DETAIL NOTES:

1. THE TOP OF BACKING CURB SHALL
TRANSITION BACK TO TOP OF SIDEWALK AT TOP
RAMP SECTION OF CURB RAMP.

SHEET DRIVEWAY LEGEND:

-  LIMITS OF 2" AC PAVING (CLASS E) FOR DRIVEWAY
-  PCC SIDEWALK (6" THICK, STANDARD FINISH), SEE NOTE 4

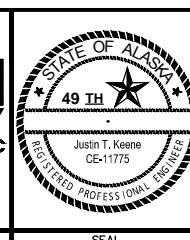
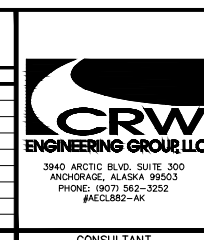


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FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
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PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	SCHED A	
<h2 style="margin: 0;">ROADWAY DETAILS</h2>			
DRIVEWAYS			
SCALE	HOR. N/A	GRID SW1638, SW1738 DATE JAN 2020	D2 of 05
	VER. N/A	STATUS 95%	SHEET



SCALE: NTS

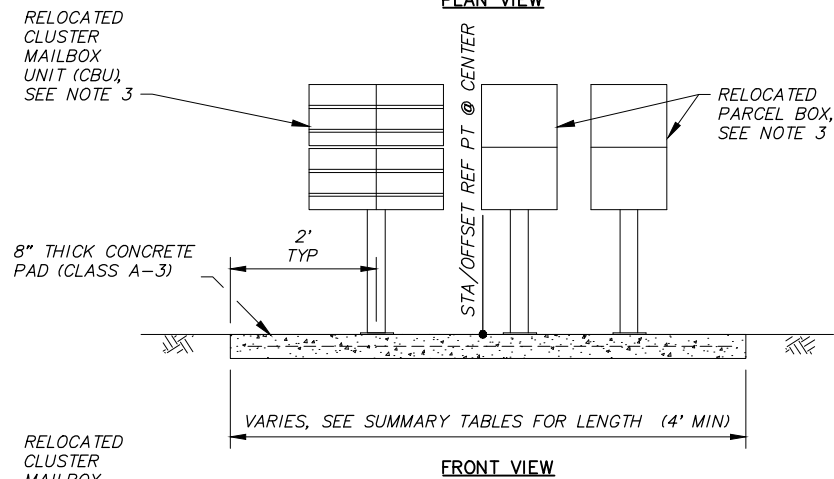


SCALE: NTS



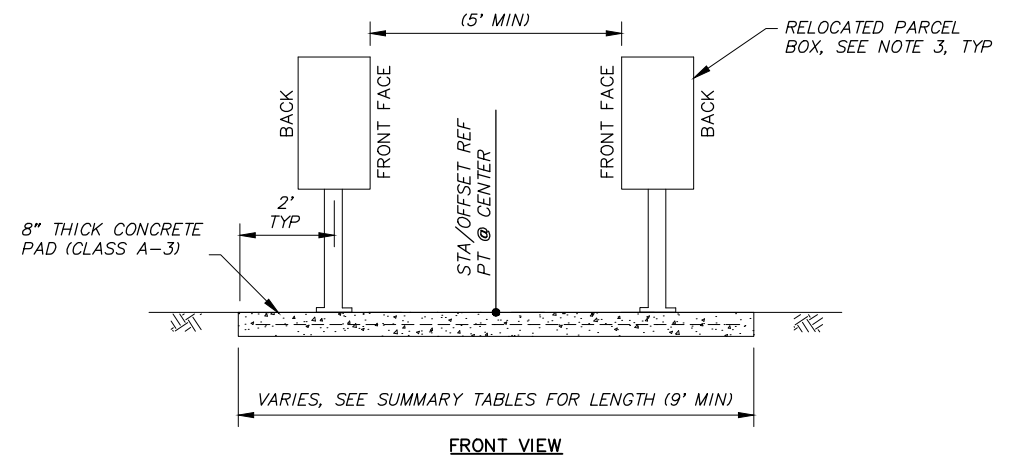
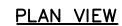
SCALE: NTS

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	SCHED A	
<h2 style="margin: 0;">ROADWAY DETAILS</h2>			
MISCELLANEOUS DETAILS			
SCALE	HOR. N/A VER. N/A	GRID SW1638, SW1738 DATE JAN 2020 STATUS OK	SHEET D3 of D5



SCALE: NTS

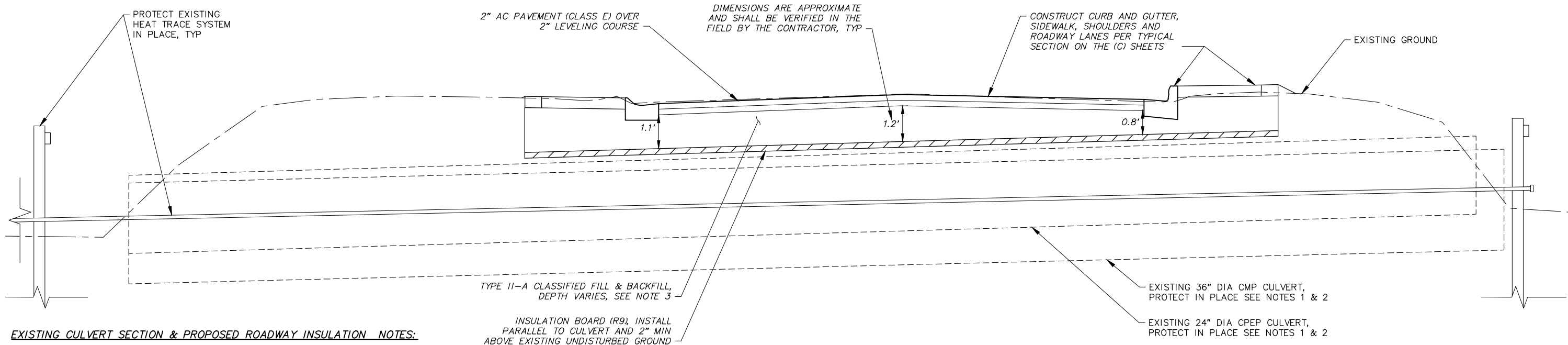
1. SEE "RELOCATE CLUSTER MAILBOX UNIT" TABLE, DEMOLITION SHEETS & ROADWAY SHEETS FOR LOCATING MAILBOXES ALONG ROADWAY. LOCATIONS ARE APPROXIMATE, VERIFY LOCATION WITH ENGINEER PRIOR TO INSTALLATION.
2. INSTALL TYPE 2A ACCESSIBLE CURB & GUTTER ALONG LENGTH OF PCC CLUSTER MAILBOX BASE WHEN THERE IS NO SIDEWALK. SEE ROADWAY SHEETS FOR CURB LAYOUT AND CURB TRANSITIONS.
3. INSTALL CLUSTER MAILBOX UNITS AND PARCEL BOXES ON CONCRETE PAD AS DIRECTED BY ENGINEER IN THE FIELD. USPS MAY REQUEST TO REPLACE EXISTING CLUSTER MAILBOX UNITS OR PARCEL BOXES, COORDINATE WITH ENGINEER PRIOR TO INSTALLATION OF RELOCATED CLUSTER MAILBOX UNITS OR PARCEL BOXES.



SCALE: NTS

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1		SCHED A
ROADWAY DETAILS			
CLUSTER MAILBOX PCC BASE DETAILS			
SCALE	HOR. N/A VER. N/A	GRID SW1636, SW1736 DATE JAN 2020	D4 of 05
		STATUS 95%	SHEET

File: I:\JobData\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\01 Civil\Phase 1\10133.00 Details - Roadway - Phase 1.dwg



EXISTING CULVERT SECTION & PROPOSED ROADWAY INSULATION NOTES:

1. CONTRACTOR SHALL PROTECT EXISTING CULVERTS IN PLACE. DURING CONSTRUCTION ACTIVITIES NEAR THE CULVERT, CONTRACTOR SHALL PROVIDE AN EXTRA AMOUNT OF COMPACTED MATERIAL (1' MIN) OVER THE EXISTING GROUND WITH RAP SURFACE OVER THE TOP OF THE CULVERTS FOR HEAVY CONSTRUCTION EQUIPMENT LOADS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE HEIGHT OF THE MATERIAL BASED UPON THE CONSTRUCTION EQUIPMENT BEING UTILIZED. COMPACTED MATERIAL AND RAP SHALL BE TRANSITIONED TO THE ADJACENT GROUND BEYOND THE CULVERTS BY PROVIDING A SMOOTH AND SAFE GRADE FOR TRAVELING PUBLIC. THIS WORK TO INSTALL AND REMOVE ADDITIONAL COMPACTED MATERIAL WITH RAP SURFACE SHALL BE INCIDENTAL TO THE PAY ITEM 70.12 TRAFFIC MAINTENANCE AND NO SEPARATE PAYMENT SHALL BE MADE.
2. CONTRACTOR SHALL REPLACE THE CULVERTS AND HEAT TRACE SYSTEM IF ANY DAMAGE IS DONE TO THE CULVERTS DURING CONSTRUCTION OPERATIONS.
3. TYPE II-A CLASSIFIED FILL AND BACKFILL ABOVE CULVERTS SHALL BE PLACED IN LAYERS NOT EXCEEDING 8" AND DENSELY COMPACTED WITH HAND OPERATED EQUIPMENT. COMPACT TO 95% MAXIMUM DENSITY AT OPTIMUM MOISTURE CONTENT, BASED ON MODIFIED PROCTOR TEST VALUES.

**EXISTING CULVERT & PROPOSED
ROADWAY INSULATION SECTION
(SECTION CUT PARALLEL TO CULVERTS)**

SCALE: NTS

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PROFILE	RB	JK
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WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
	GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
STAKING							
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							
VERTICAL DATUM							
REVISIONS							

CRW ENGINEERING GROUP LLC

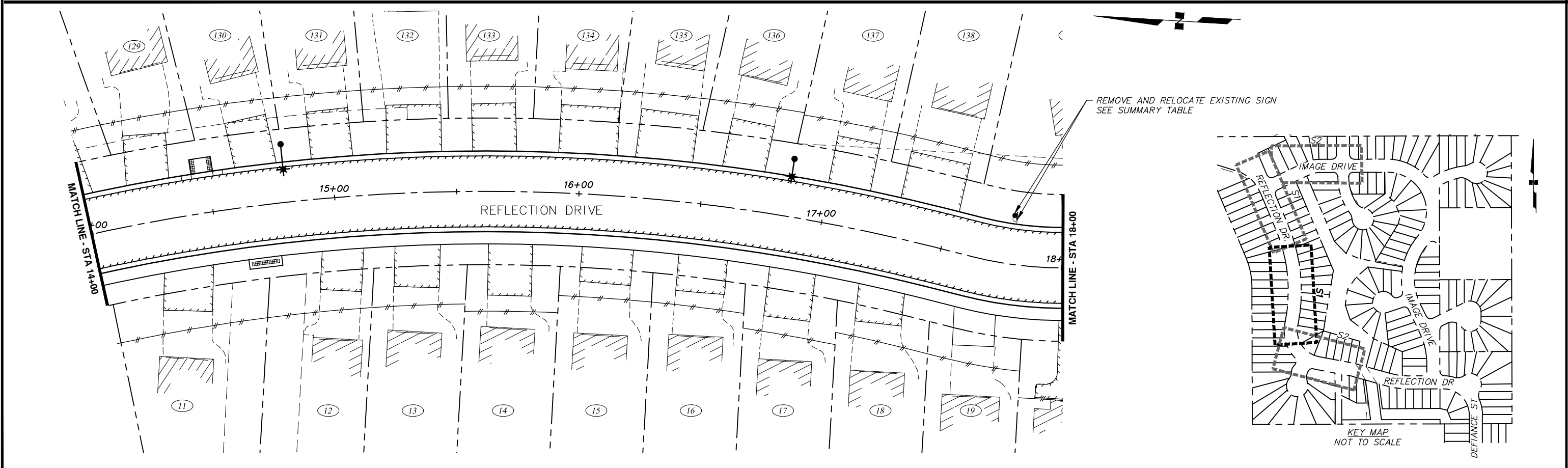
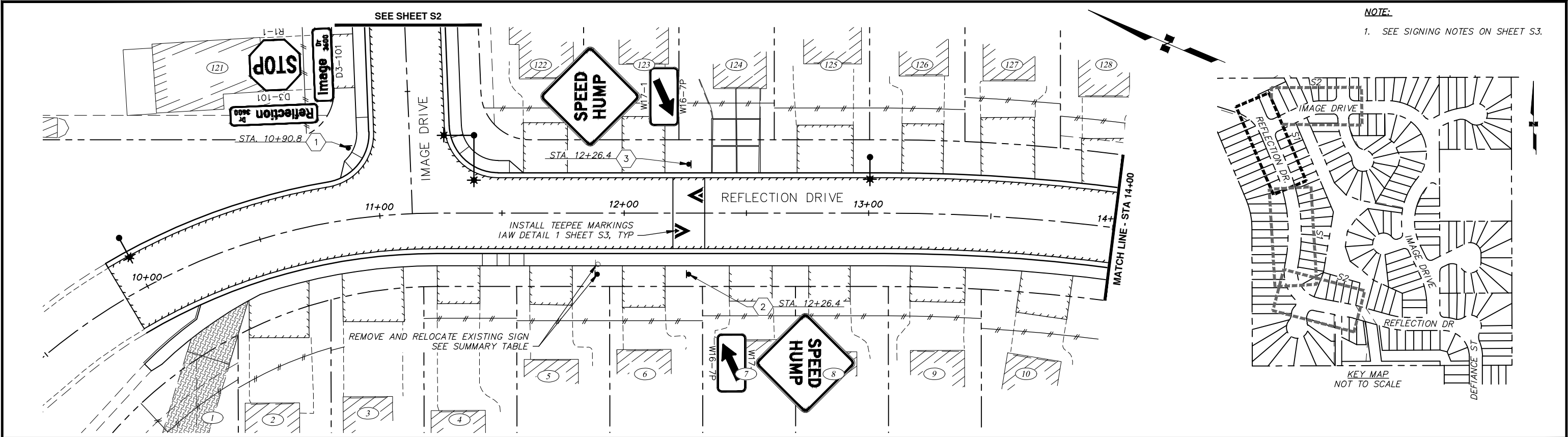
3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECLE882-AK

STATE OF ALASKA
49 TH
Justin T. Keane
CE-11775
REGISTERED PROFESSIONAL ENGINEER



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	SCHED A	
ROADWAY DETAILS			
EXISTING CULVERT & PROPOSED ROADWAY INSULATION SECTION			
SCALE HOR. N/A VER. N/A	GRID SW1638, SW1738 DATE JAN 2020	STATUS 95%	SHEET D5 of D5

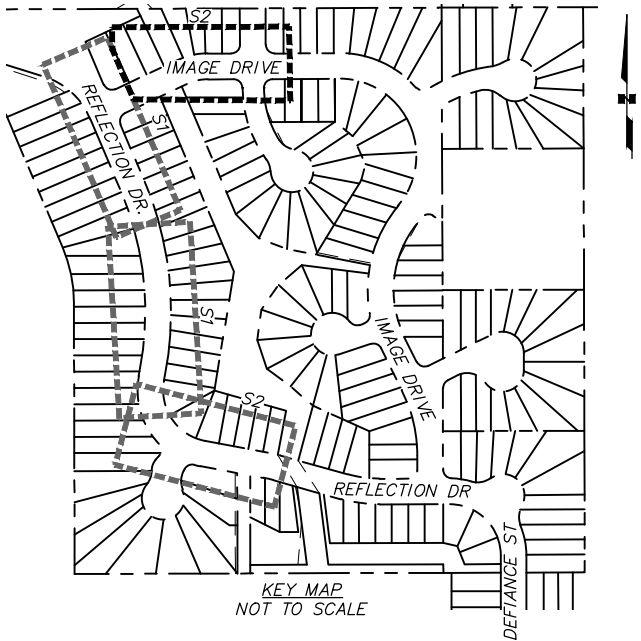
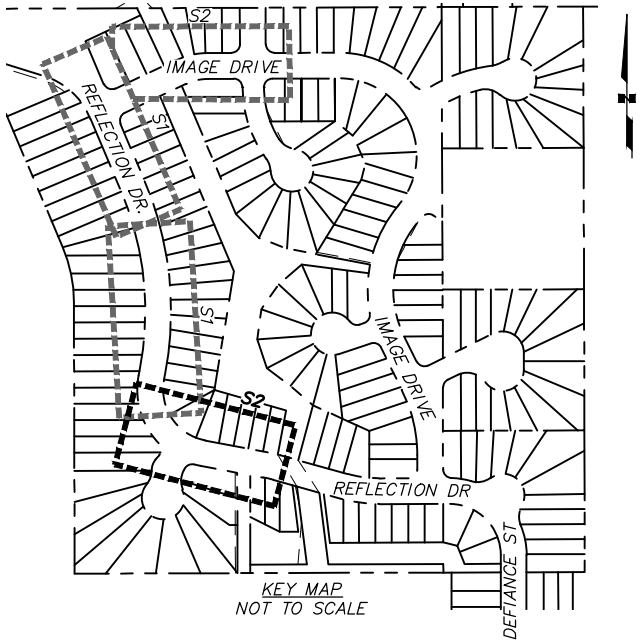
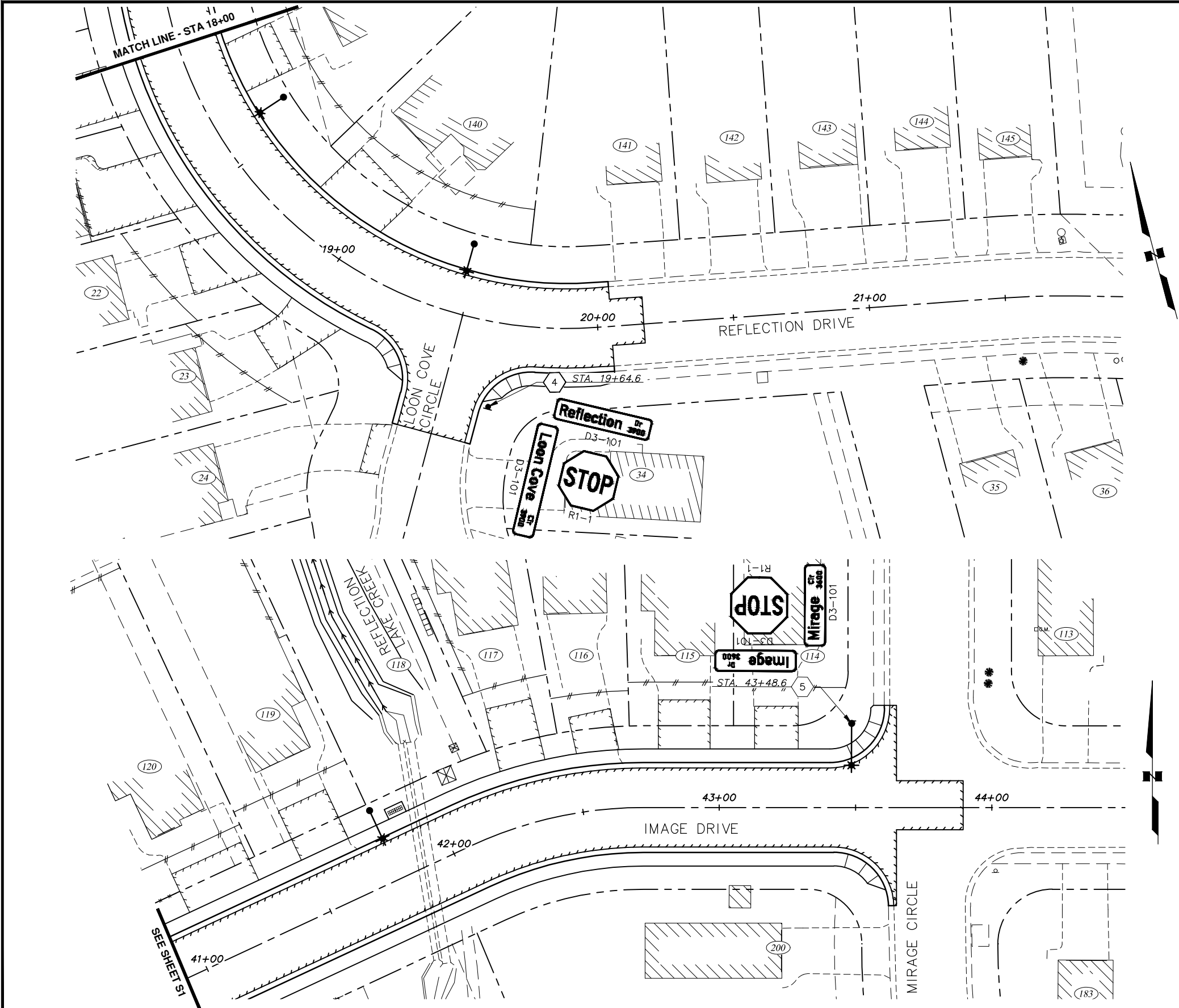
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RECORD DRAWING 1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ DATE: _____ BY: _____		DATA BASE TS MJ TOPOGRAPHY TS MJ PROFILE RB JK STORM SEWER MV JH WATER/SANITARY SEWER MV JK GAS MV JK TELEPHONE MV JK ELECTRIC JH TK DESIGN RB JK QUANTITIES RB JK PRELIMINARY/FINAL RB JK MUNICIPAL/STATE RB JK		DRAWN BY TS MJ RB JK MV JH MV JK JH TK RB JK RB JK RB JK		CHECKED BY MJ JK JK JK JK JK JK JK		PLAN CHECK			
2. DATA TRANSFERRED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____		3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ BY: _____		FIELD BOOKS DESIGN CRW Books 147, 148, & 151 STAKING ASBUILT CONTRACTOR INSPECTOR		BM NO. GAAB 66 See page D-24 of the MOA Benchmark Book GAAB 96 See page D-26 of the MOA Benchmark Book		ELEV. 238.10 313.83		REV DATE DESCRIPTION BY	
GRAPHIC SCALE 40 20 0 20 40		BASIS OF THIS DATUM GAAB 1972 ADJUST		REVISIONS		CONSULTANT		SEAL		PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT 14-50 IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1 SIGNING & STRIPING REFLECTION DRIVE - BOP TO STA 18+00 SCALE HOR. 1"=20' VER. N/A GRID SW1638, SW1738 DATE JAN 2020 STATUS 95% SHEET S1 of S3	

File: s:_data\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\01 Civil\Phase 1\0133.00 Signing & Striping - Phase 1.dwg

NOTE:
1. SEE SIGNING NOTES ON SHEET S3.



RECORD DRAWING

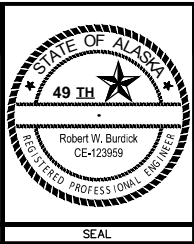
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THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
CONTRACTOR: _____ DATE: _____
BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
DATA TRANSFER CHECKED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____
BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

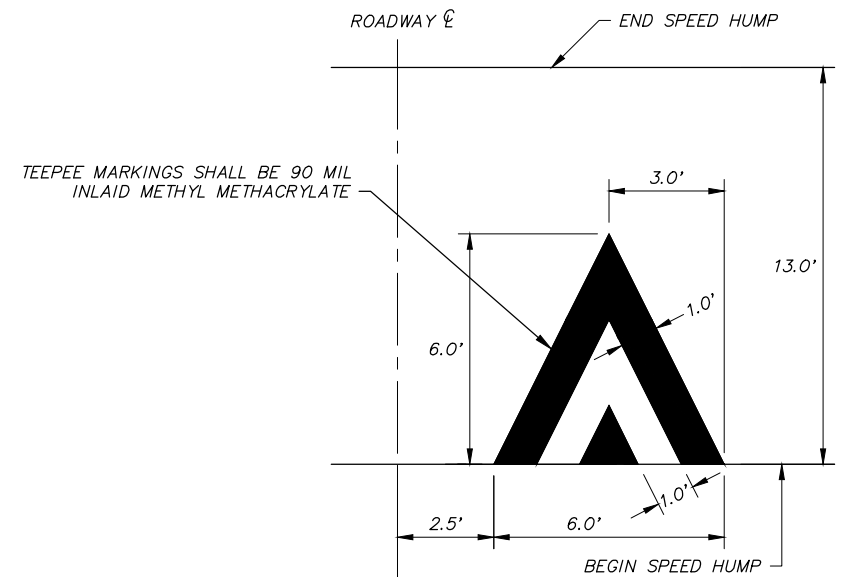
FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151		GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
		GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
STAKING								
ASBUILT								
CONTRACTOR								
INSPECTOR								
PLAN CHECK		BASIS OF THIS DATUM GAAB 1972 ADJUST						
CONSTRUCTION RECORD								
VERTICAL DATUM								
REVISIONS								



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1		SCHED A
SIGNING & STRIPING			
REFLECTION DRIVE - STA 18+00 TO EOP IMAGE DRIVE - STA 41+00 TO EOP			
SCALE	HOR. 1"=20' VER. N/A	GRID SW1638, SW1738	S2 of S3
		DATE JAN 2020	
		SHEET	

1. THE STATIONS INDICATED IN THE SIGN SUMMARY ARE APPROXIMATE. INSTALL SIGNS AND SIGN FOUNDATIONS PER MASS STANDARD DETAILS. BEFORE INSTALLING ANY SIGN, STAKE THE LOCATION OF ALL SIGNS FOR THE ENGINEER'S REVIEW AND APPROVAL.
2. PROVIDE PERFORATED STEEL TUBE (PST) SIGN POSTS OF THE SIZE INDICATED IN THE SIGN SUMMARY.
3. INSTALL THE POSTS FOR STOP SIGNS AT LOCATIONS THAT CONFORM TO MASS STANDARD DETAIL 70-18.
4. ALL STOP SIGNS AND STREET NAME SIGNS SHALL REMAIN OPERATIONAL DURING CONSTRUCTION.
5. INSTALL SIGN ON LIGHT POLES PER SEE MASS STANDARD DETAIL 70-30.
6. THE LETTERING FOR STREET NAME SIGNS (D3 SERIES) SHALL BE FEDERAL HIGHWAY ADMINISTRATION "FHWA 2000 SERIES C" LETTERING, A COMBINATION OF LOWER-CASE LETTERS WITH INITIAL UPPER-CASE LETTERS.

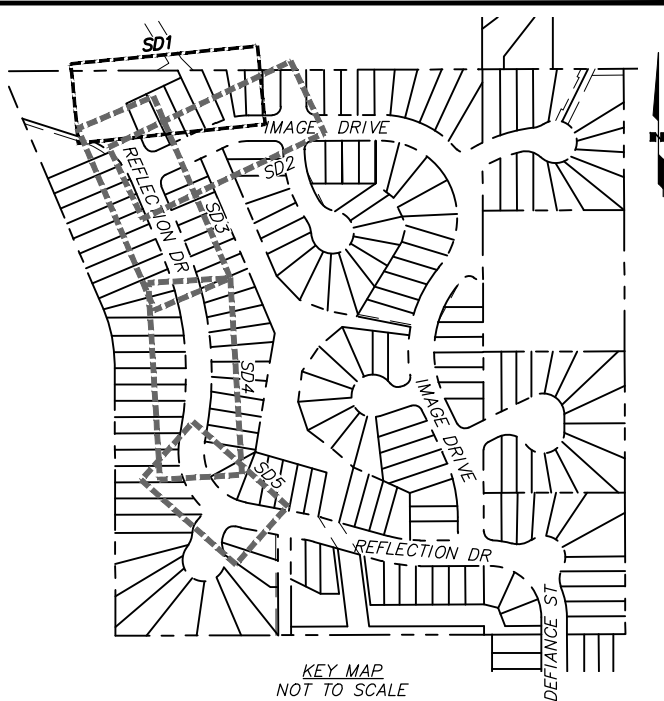
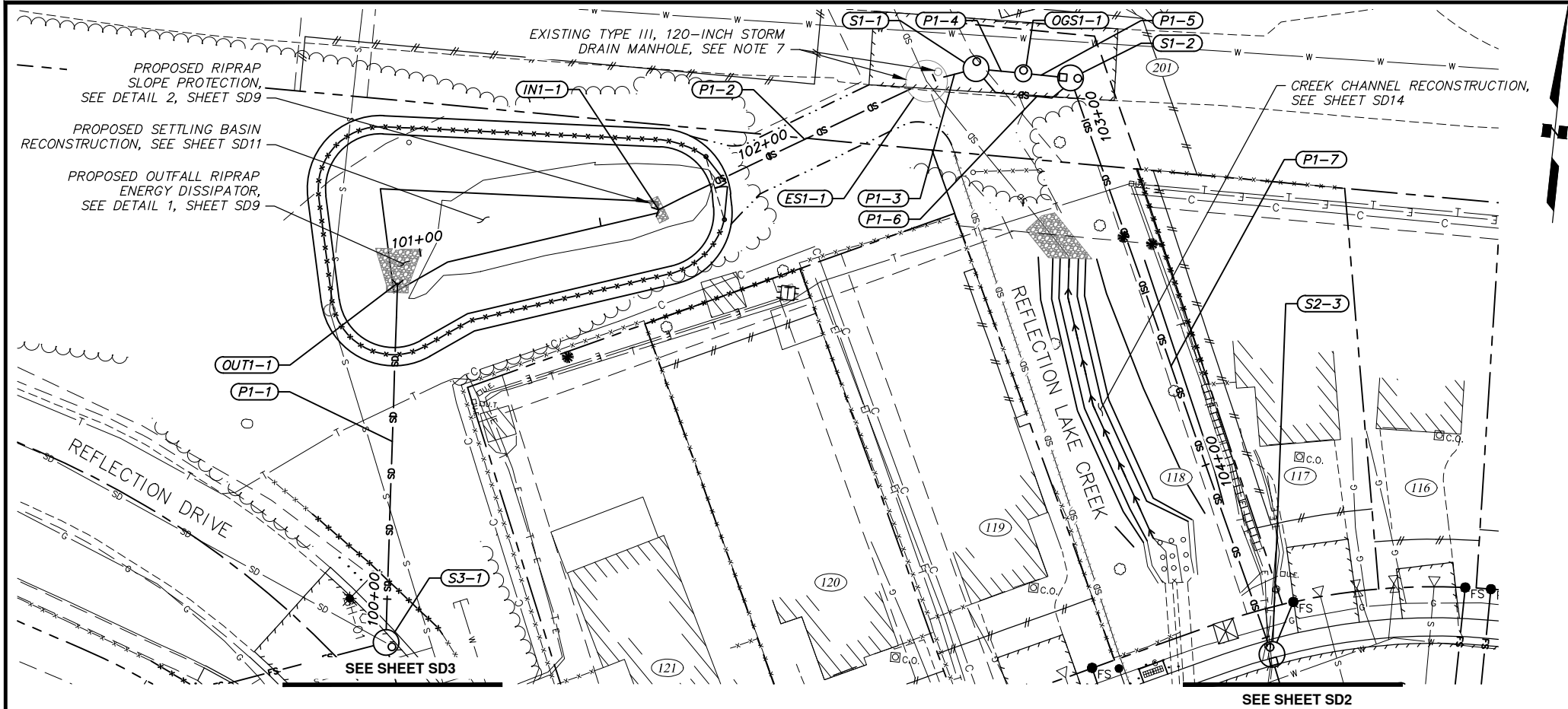
70.11	STANDARD SIGN										
SHEET	POST NO.	STATION	OFFSET	TYPE	LEGEND	WIDTH (INCHES)	HEIGHT (INCHES)	AREA (SF)	SIGN FACES	SIGN POST	REMARKS
S1	1	10+90.8	29.0 LT	D3-101	REFLECTION DR 3600	36	8	2.00	NE/SW	2.5" PST	ONE DOUBLE SIDED PANEL
				D3-101	IMAGE DR 3600	30	8	1.67	NW/SE		ONE DOUBLE SIDED PANEL
				R1-1	STOP	30	30	6.25	NE		
	2	12+26.4	25.0 RT	W17-1	SPEED HUMP	30	30	6.25	NW	2.5" PST	
				W16-7P	ARROW (LEFT)	24	12	2.00	NW		
	3	12+26.4	20.0 LT	W17-1	SPEED HUMP	30	30	6.25	SE	2.5" PST	
				W16-7P	ARROW (LEFT)	24	12	2.00	SE		
S2	4	19+64.6	31.0 RT	D3-101	REFLECTION DR 3900	36	8	2.00	NE/SW	2.5" PST	ONE DOUBLE SIDED PANEL
				D3-101	LOON COVE CIR 3900	42	8	2.33	NW/SE		ONE DOUBLE SIDED PANEL
				R1-1	STOP	30	30	6.25	SW		
	5	43+48.6	30.8 LT	D3-101	IMAGE DR 3600	30	8	1.67	N/S	MOUNT ON LIGHT POLE	ONE DOUBLE SIDED PANEL
				D3-101	MIRAGE CIR 3600	30	8	1.67	E/W		ONE DOUBLE SIDED PANEL
				R1-1	STOP	30	30	6.25	N		



1

1

[illegible]

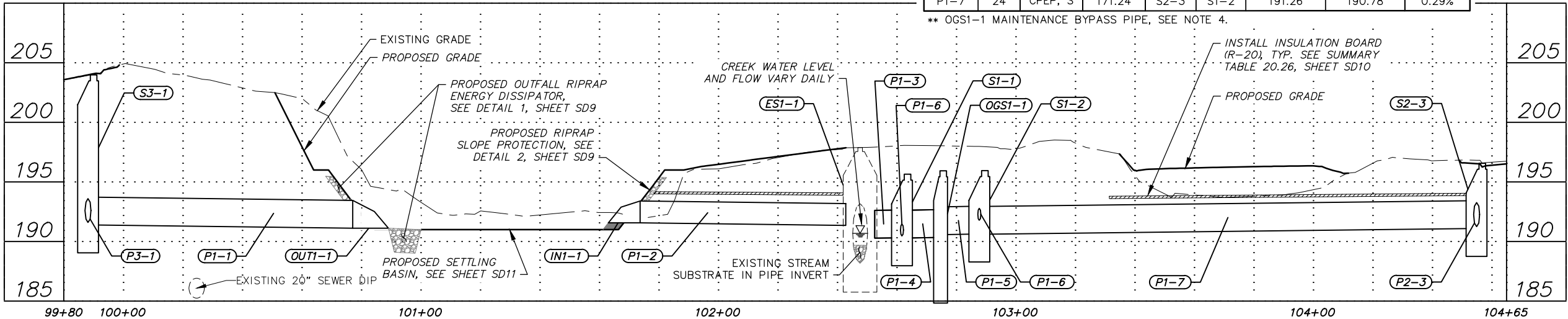


- NOTES:**
- AN ASTERISK (*) DENOTES PIPE OR STRUCTURE NOT SHOWN IN PROFILE FOR CLARITY.
 - REFER TO SHEET SD6 FOR GENERAL STORM DRAIN STRUCTURE/PIPE NOTES AND STRUCTURE ABBREVIATIONS USED ON SUMMARY TABLES SHOWN ON THIS SHEET.
 - REFER TO SHEETS SD6-SD9 FOR STORM DRAIN DETAILS.
 - CPEP FITTINGS I.A.W. MASS SECTION 55.002 SHALL BE USED FOR BYPASS PIPING, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. LOCATION OF FITTINGS SHOWN ARE APPROXIMATE. CONTRACTOR SHALL FIELD LOCATE FITTINGS WITH ENGINEER'S APPROVAL TO MINIMIZE CONFLICTS WITH OTHER UTILITIES AND OBSTRUCTIONS. CONCRETE THRUST BLOCKS I.A.W. MASS STANDARD DETAIL 60-06 SHALL BE INSTALLED AT ALL FITTINGS. PAYMENT FOR THRUST BLOCKS SHALL BE CONSIDERED INCIDENTAL TO PAY ITEM 55.02, FURNISH, INSTALL, AND TELEVIEW PIPE.
 - CAUTION!!! THE LOCATION OF EXISTING UTILITY CROSSINGS SHOWN IN PROFILE ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY LOCATION OF ALL UTILITIES ENCOUNTERED AND RECORD THEIR LOCATION ON THE CONTRACT RECORD DRAWINGS. DISCREPANCIES SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION TO THE ENGINEER. SEE SPECIFICATIONS FOR UTILITY RELOCATION SUMMARY.
 - REFER TO SHEET SD7 FOR OGS AND BYPASS STRUCTURE DETAILS.
 - REMOVE & RESET MANHOLE REDUCING SLAB AND REMOVE & REPLACE MANHOLE RUNGS IF NECESSARY TO INSTALL PIPES P1-2 & P1-3. PLACE NEW RUNGS ON UNOBSTRUCTED SIDE OF MANHOLE (SW) PER MASS STANDARD DETAIL 55-4 AND ROTATE REDUCING SLAB ACCORDINGLY. WORK SHALL BE PAID FOR UNDER PAY ITEM 55.28, EXISTING MANHOLE MODIFICATIONS.

55.04 & 55.05 - STORM DRAIN STRUCTURES							
STRUCTURE ID	TYPE OF STRUCTURE	TYPE OF CASTING	NORTHING	EASTING	TOP OF CASTING ELEVATION	CURB TYPE	COMMENTS
OUT1-1	END SECTION	N/A	329580.86	367659.75	-	N/A	24" CPEP END SECTION
IN1-1	END SECTION	N/A	329608.33	367728.89	-	N/A	18" CPEP END SECTION
ES1-1	CONNECT	MH	329650.40	367798.49	-	N/A	EXISTING MH (TYPE III, 120-INCH)
S1-1	MH II	MH	329655.11	367811.60	197.99	N/A	
OGS1-1	OGS	MH	329655.11	367824.60	198.02	N/A	SEE DETAIL 2, SHEET SD7
S1-2	BYPASS MH II	MH	329655.11	367837.60	197.95	N/A	SEE DETAIL 3, SHEET SD7

55.02 - STORM DRAIN PIPE								
PIPE NAME	SIZE (IN.)	PIPE TYPE	LENGTH (FT.)	FROM	TO	INLET ELEVATION	OUTLET ELEVATION	SLOPE
P1-1	24	CPEP, S	89.05	S3-1	OUT1-1	191.55	191.28	0.31%
P1-2	18	CPEP, S	76.64	IN1-1	ES1-1	191.73	191.50	0.32%
P1-3	24	CPEP, S	14.09	S1-1	ES1-1	190.47	190.45	0.33%
P1-4	24	CPEP, S	13.00	OGS1-1	S1-1	190.50	190.47	0.38%
P1-5	24	CPEP, S	13.00	S1-2	OGS1-1	190.78	190.75	0.37%
P1-6**	24	CPEP, S	31.51	S1-2	S1-1	191.78	190.47	5.14%
P1-7	24	CPEP, S	171.24	S2-3	S1-2	191.26	190.78	0.29%

** OGS1-1 MAINTENANCE BYPASS PIPE, SEE NOTE 4.



RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____ DATE: _____

BY: _____ TITLE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
	GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				

STAKING

ASBUILT

CONTRACTOR

INSPECTOR

PLAN CHECK

CONSTRUCTION RECORD

VERTICAL DATUM

REVISIONS

CONSULTANT

SEAL

CRW ENGINEERING GROUP LLC

3940 ARCTIC BLVD, SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AEC0882-AK

STATE OF ALASKA

49 TH

Joseph C. Hegira
CE-11770

REGISTERED PROFESSIONAL ENGINEER

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

14-50 IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1 SCHED B

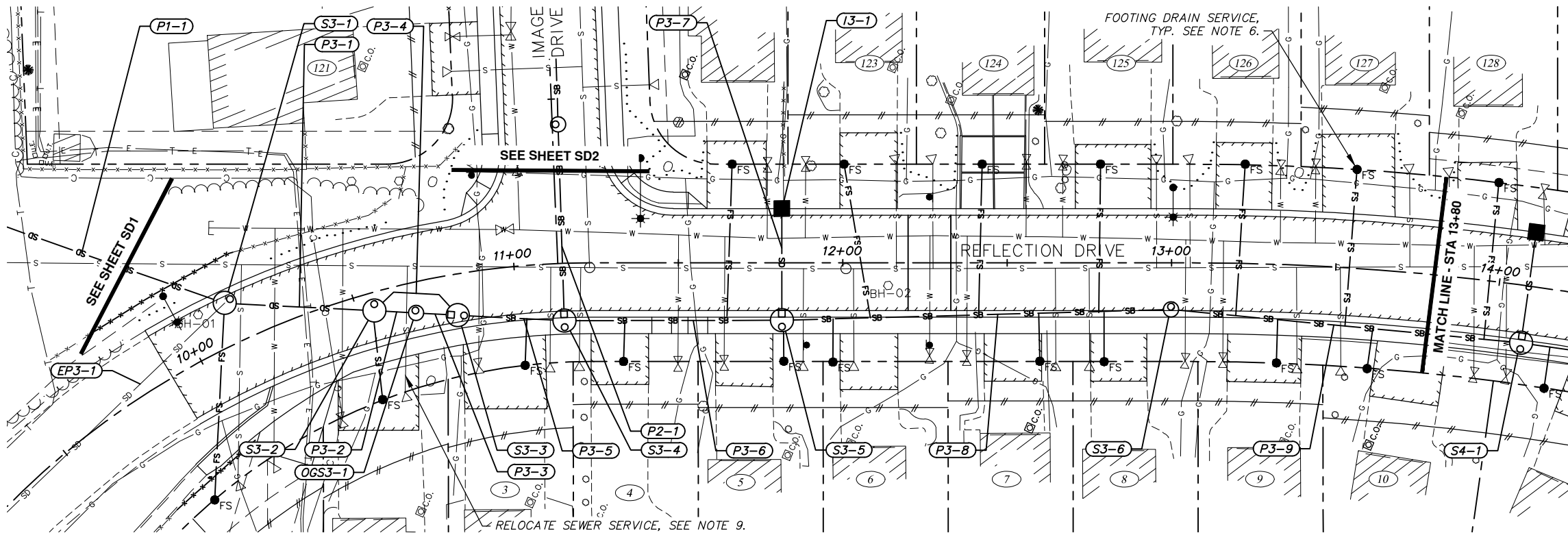
STORM DRAIN PLAN & PROFILE

SCALE HOR. 1"=20'
VER. 1"=5'

GRID SW1638, SW1738

DATE JAN 2020 STATUS 95% SHEET SD1 of SD15

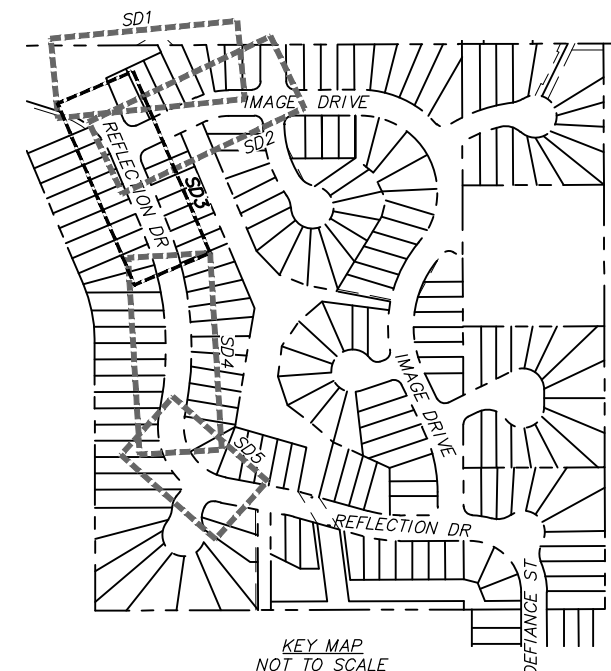
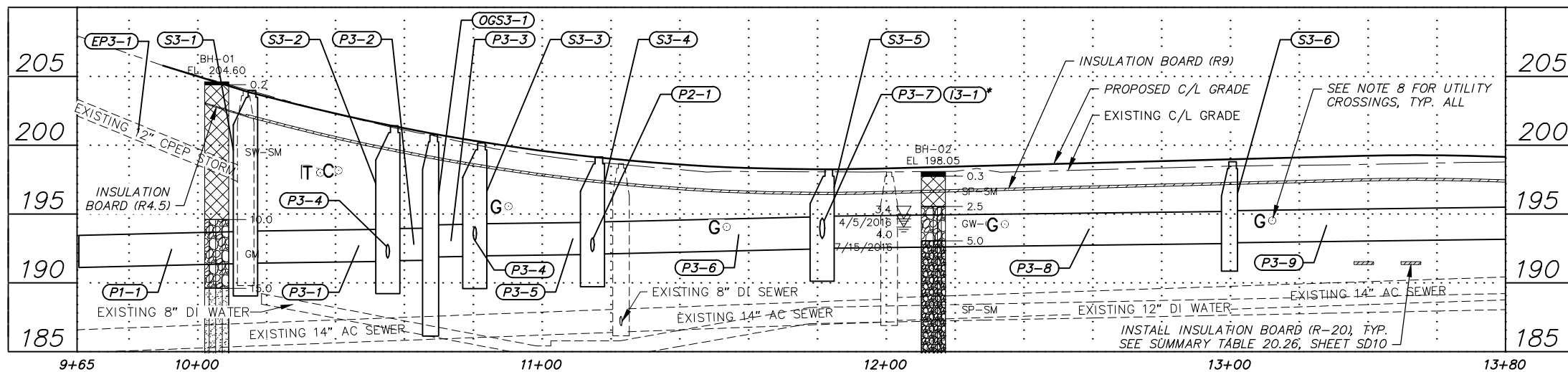
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55.04, 55.05, 55.09 & 55.22 – STORM DRAIN STRUCTURES							
STRUCTURE ID	TYPE OF STRUCTURE	TYPE OF CASTING	STATION	OFFSET TO STRUCTURE C/L	TOP OF CASTING ELEVATION	CURB TYPE	COMMENTS
S3-1	MH II/CONNECT	MH	10+13.90	12.64' LT	203.81	N/A	CONNECT EX. PIPE (EP3-1)
S3-2	MH II	MH	10+55.24	5.50' RT	201.37	N/A	
OGS3-1	OGS	MH	10+67.71	9.30' RT	200.72	N/A	SEE DETAIL 1, SHEET SD8
S3-3	BYPASS MH II	MH	10+80.56	12.43' RT	200.03	N/A	SEE DETAIL 2, SHEET SD8
S3-4	CB MH II	CI	11+14.72	17.45' RT	199.31	2	
S3-5	CB MH II	CI	11+81.48	17.45' RT	198.41	2	
I3-1	CB	CI	11+81.49	16.50' LT	198.05	2	
S3-6	MH I	MH	12+99.80	14.19' RT	198.76	N/A	

55.02 & 55.03 – STORM DRAIN & SUBDRAIN PIPE								
PIPE NAME	SIZE (IN.)	PIPE TYPE	LENGTH (FT.)	FROM	TO	INLET ELEVATION	OUTLET ELEVATION	SLOPE
EP3-1	12	CPEP	—	—	S3-1	—	197.4±	—
P3-1	24	CPEP, SP	45.64	S3-2	S3-1	191.72	191.60	0.30%
P3-2	24	CPEP, S	12.67	OGS3-1	S3-2	191.80	191.77	0.39%
P3-3	24	CPEP, S	12.62	S3-3	OGS3-1	192.08	192.05	0.39%
P3-4**	12	CPEP, S	30.42	S3-3	S3-2	193.08	191.80	5.24%
P3-5	24	CPEP, SP	32.49	S3-4	S3-3	192.21	192.13	0.30%
P3-6	24	CPEP, SP	66.28	S3-5	S3-4	192.62	192.26	0.60%
P3-7	12	CPEP, S	33.91	I3-1	S3-5	193.31	193.22	0.31%
P3-8	24	CPEP, SP	118.46	S3-6	S3-5	193.02	192.67	0.31%
P3-9	24	CPEP, SP	107.11	S4-1	S3-6	193.42	193.07	0.34%

** OGS3-1 MAINTENANCE BYPASS PIPE, SEE NOTE 5.



RECORD DRAWING
1. DATA PROVIDED BY: _____ TITLE: _____
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
CONTRACTOR: _____
BY: _____ TITLE: _____ DATE: _____
2. DATA TRANSFERRED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
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COMPANY: _____ DATE: _____
BY: _____

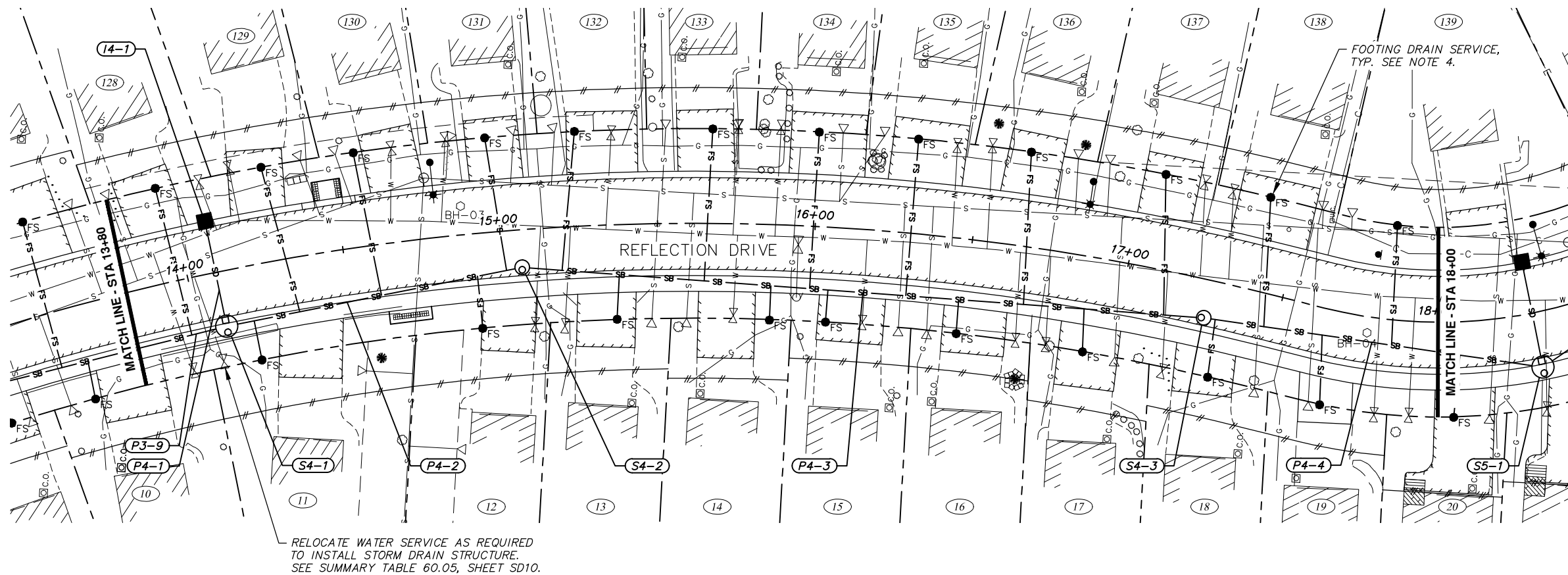
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BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151		GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
		GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
STAKING								
ASBUILT								
CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM GAAB 1972 ADJUST								
PLAN CHECK								
CONSTRUCTION RECORD								
VERTICAL DATUM								
REVISIONS								
CONSULTANT								
SEAL								



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1		SCHED B
STORM DRAIN PLAN & PROFILE			
SCALE	HOR. 1"=20' VER. 1"=5'	GRID SW1638, SW1738	SD3 of SD15
		DATE JAN 2020	
		SHEET	

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NOTES:

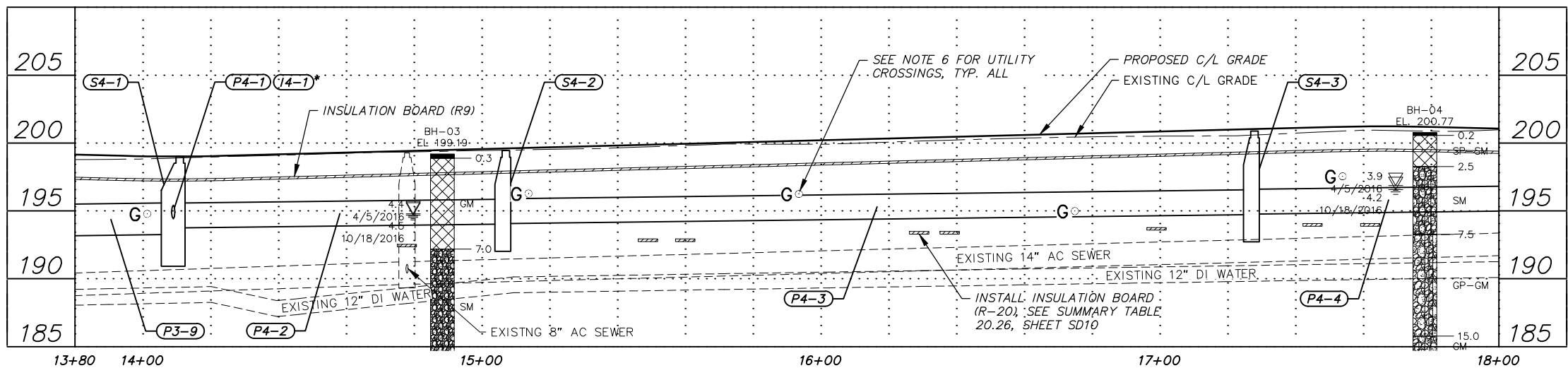
1. AN ASTERISK (*) DENOTES PIPE OR STRUCTURE NOT SHOWN IN PROFILE FOR CLARITY.
2. REFER TO SHEET SD6 FOR GENERAL STORM DRAIN STRUCTURE/PIPE NOTES AND STRUCTURE ABBREVIATIONS USED ON SUMMARY TABLES SHOWN ON THIS SHEET.
3. REFER TO SHEETS SD6-SD9 FOR STORM DRAIN DETAILS.
4. REFER TO SHEET SD10 FOR FOOTING DRAIN SERVICE SUMMARY TABLE.
5. FOOTING DRAIN SERVICES NOT SHOWN IN PROFILE FOR CLARITY.
6. CAUTION!!! THE LOCATION OF EXISTING UTILITY CROSSINGS SHOWN IN PROFILE ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY LOCATION OF ALL UTILITIES ENCOUNTERED AND RECORD THEIR LOCATION ON THE CONTRACT RECORD DRAWINGS. DISCREPANCIES SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION TO THE ENGINEER. SEE SPECIFICATIONS FOR UTILITY RELOCATION SUMMARY.

55.05 & 55.09 - STORM DRAIN STRUCTURES

STRUCTURE ID	TYPE OF STRUCTURE	TYPE OF CASTING	STATION	OFFSET TO STRUCTURE C/L	TOP OF CASTING ELEVATION	CURB TYPE	COMMENTS
S4-1	CB MH II	CI	14+08.99	17.45' RT	199.17	2	
I4-1	CB	CI	14+08.99	16.50' LT	198.82	2	
S4-2	MH I	MH	15+06.23	11.91' RT	199.40	N/A	
S4-3	MH I	MH	17+26.99	12.10' RT	200.83	N/A	

55.02 & 55.03 - STORM DRAIN & SUBDRAIN PIPE

PIPE NAME	SIZE (IN.)	PIPE TYPE	LENGTH (FT.)	FROM	TO	INLET ELEVATION	OUTLET ELEVATION	SLOPE
P4-1	12	CPEP, S	34.13	I4-1	S4-1	194.52	194.42	0.34%
P4-2	18	CPEP, SP	95.49	S4-2	S4-1	194.19	193.92	0.30%
P4-3	18	CPEP, SP	216.48	S4-3	S4-2	194.88	194.24	0.30%
P4-4	18	CPEP, SP	108.69	S5-1	S4-3	195.24	194.93	0.30%



RECORD DRAWING

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CONTRACTOR: _____
BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____

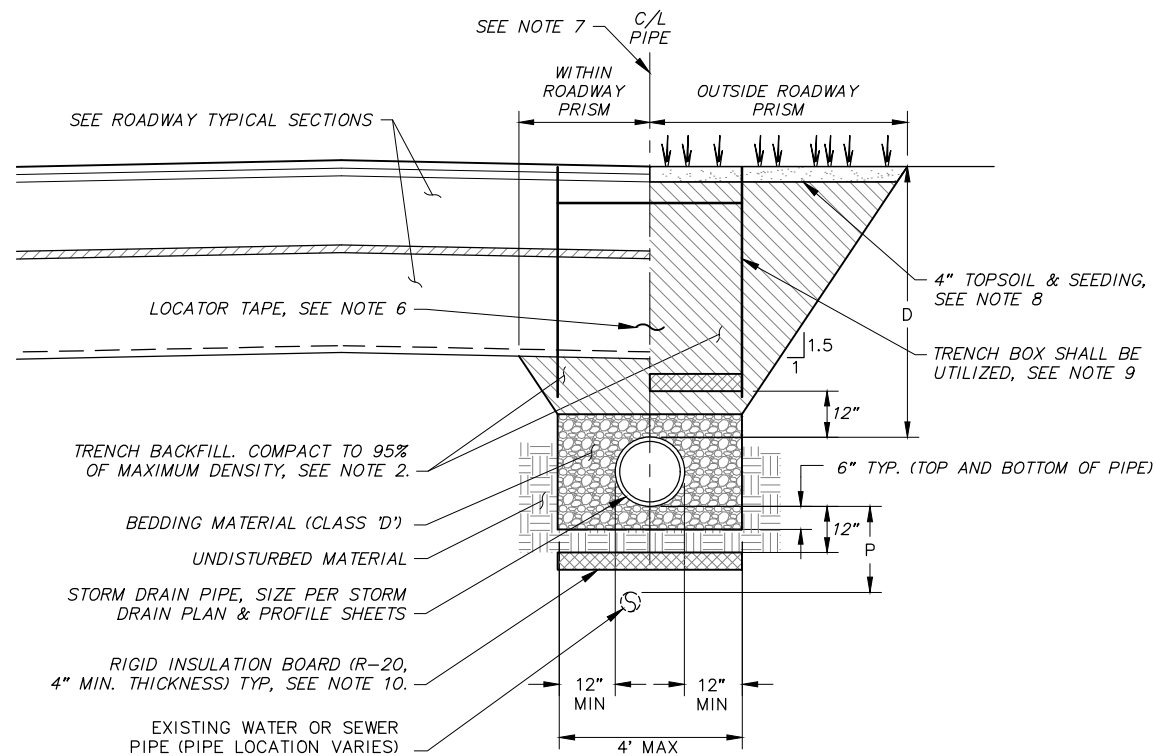
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
DATA TRANSFER CHECKED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____
BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

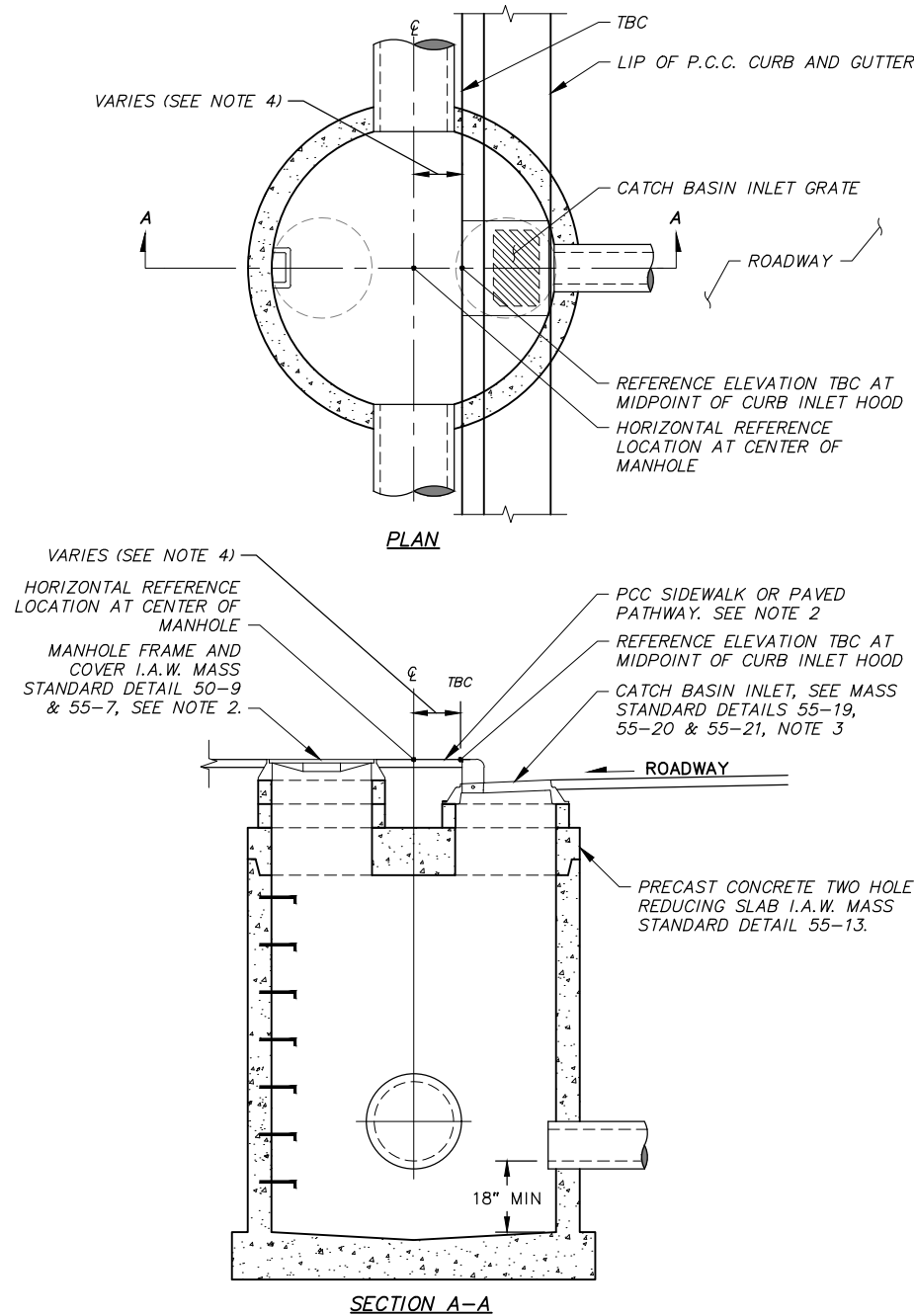
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DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
	GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
STAKING							
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	SCHED B	
STORM DRAIN PLAN & PROFILE			
SCALE HOR. 1"=20' VER. 1"=5'	GRID SW1638, SW1738 DATE JAN 2020	STATUS 95%	SHEET SD4 of SD15



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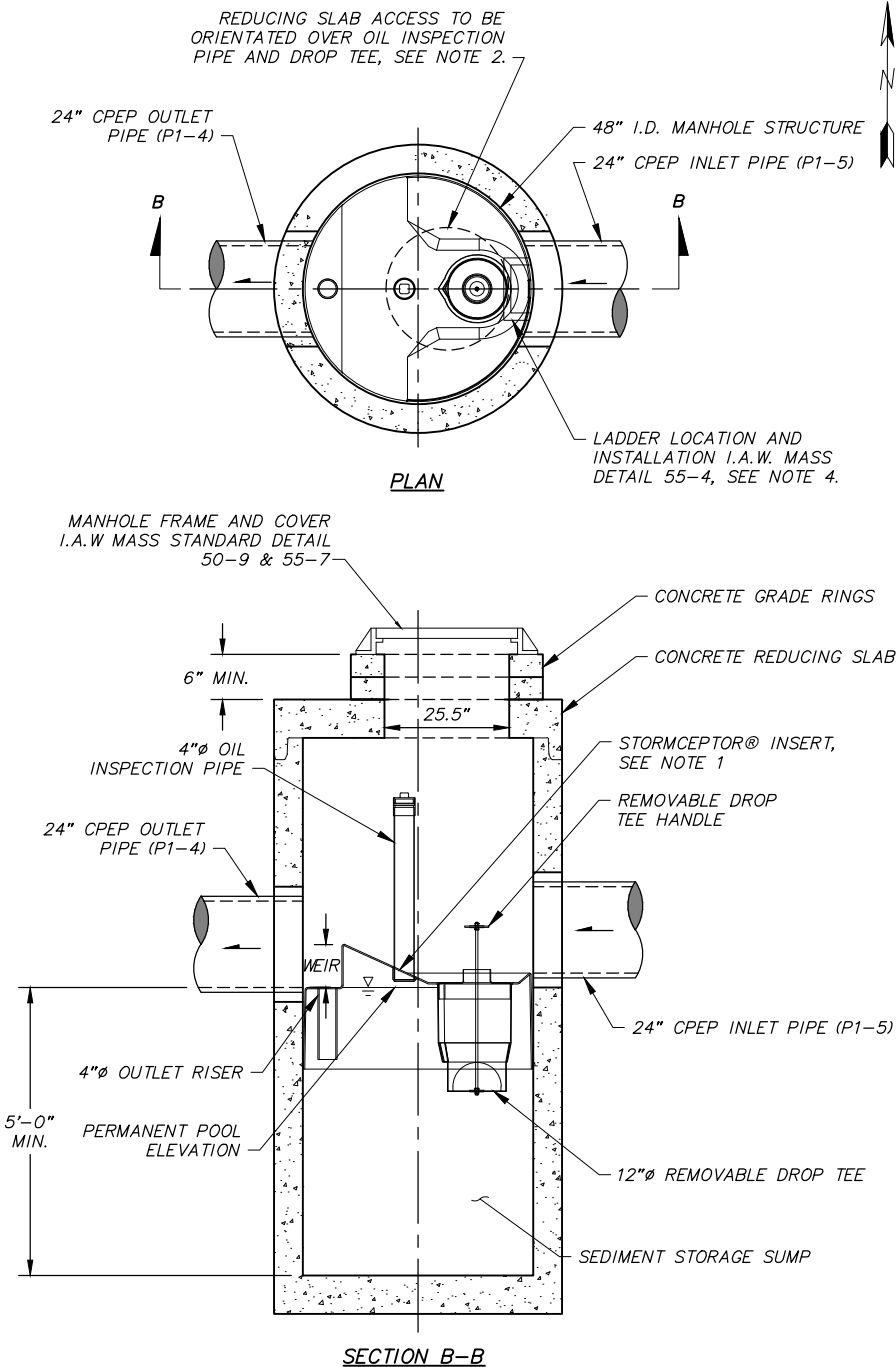


TYPE II CATCH BASIN MANHOLE NOTES

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MUNICIPALITY OF ANCHORAGE STANDARD SPECIFICATIONS AS CURRENTLY AMENDED AND AS MODIFIED ON THIS DETAIL.
- SET MANHOLE COVER 1/4-INCH BELOW PCC SIDEWALK OR PAVED PATHWAY FINISH GRADE OR PER MASS STANDARD DETAIL 55-10 FOR ALL OTHER LOCATIONS.
- MH CENTER MAY BE ON ROADWAY SIDE OF CURB LINE IN SOME LOCATIONS. ALIGN CATCH BASIN INLET WITH CURB LINE.
- OFFSET FOR STANDARD INSTALLATION IS 0.95'.

1 TYPE II CATCH BASIN MANHOLE DETAIL

SCALE: NTS

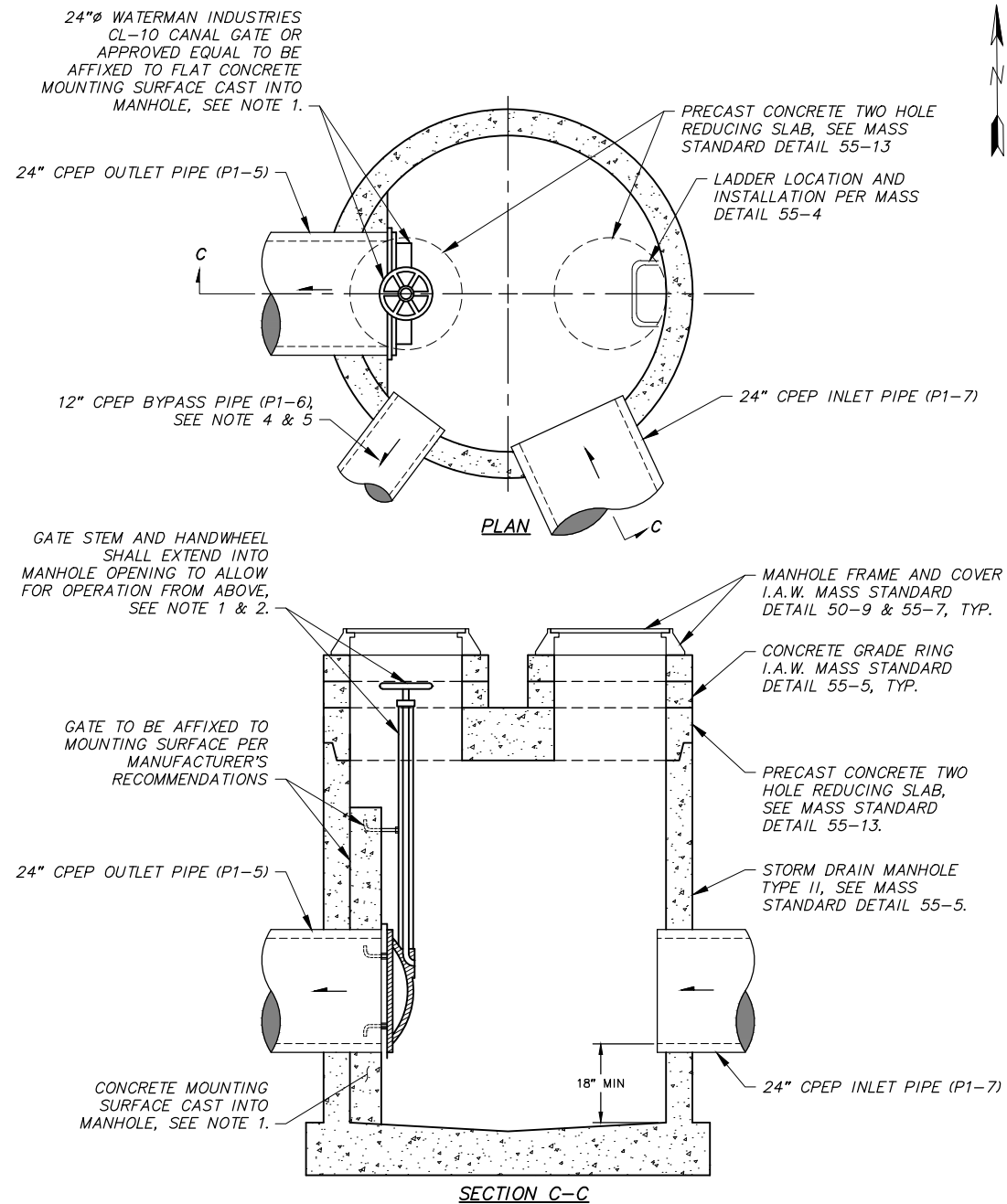


OIL & GRIT SEPARATOR NOTES

- OIL AND GRIT SEPARATOR (STRUCTURE OGS1-1) SHALL BE STORMCEPTOR MODEL STC450i MANUFACTURED BY CONTECH ENGINEERED SOLUTIONS LLC OR APPROVED EQUAL.
- ACCESS OPENING THROUGH REDUCING SLAB SHOULD BE POSITIONED OVER THE DROP TEE AND OIL PORT.
- SEE STORM DRAIN PLAN & PROFILE SHEETS FOR INLET AND OUTLET PIPE INVERTS & ORIENTATION AND STRUCTURE INFORMATION.
- LADDER RUNGS NOT SHOWN IN SECTION VIEW FOR CLARITY.

2 OIL AND GRIT SEPARATOR (OGS1-1) DETAIL

SCALE: NTS



BYPASS MANHOLE NOTES

- CAST CONCRETE MOUNTING SURFACE INTO MANHOLE SUCH THAT BYPASS GATE HANDWHEEL IS CENTERED IN ACCESS OPENING.
- BYPASS GATE STEM SHALL BE NON-RISING TO POSITION HANDWHEEL AT CONVENIENT STATIC OPERATING ELEVATION FROM MANHOLE OPENING ABOVE.
- BYPASS MANHOLE (STRUCTURE S1-2) SHALL BE PAID FOR UNDER PAY ITEM 55.05 CONSTRUCT (TYPE II) BYPASS MANHOLE.
- BYPASS PIPE (P1-6) NOT SHOWN IN SECTION A-A FOR CLARITY.
- ADJUST LOCATION OF PIPE PENETRATION INTO MANHOLE FOR BYPASS PIPE (P1-6) AS REQUIRED TO AVOID CONFLICT WITH CONCRETE MOUNTING SURFACE.

3 BYPASS MANHOLE (S1-2) DETAIL

SCALE: NTS

RECORD DRAWING	
1. DATA PROVIDED BY: _____ TITLE: _____	
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.	
CONTRACTOR: _____ DATE: _____	
BY: _____	
2. DATA TRANSFERRED BY: _____ TITLE: _____	
COMPANY: _____ DATE: _____	
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.	
DATA TRANSFER CHECKED BY: _____ TITLE: _____	
COMPANY: _____ DATE: _____	
BY: _____	

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
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ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

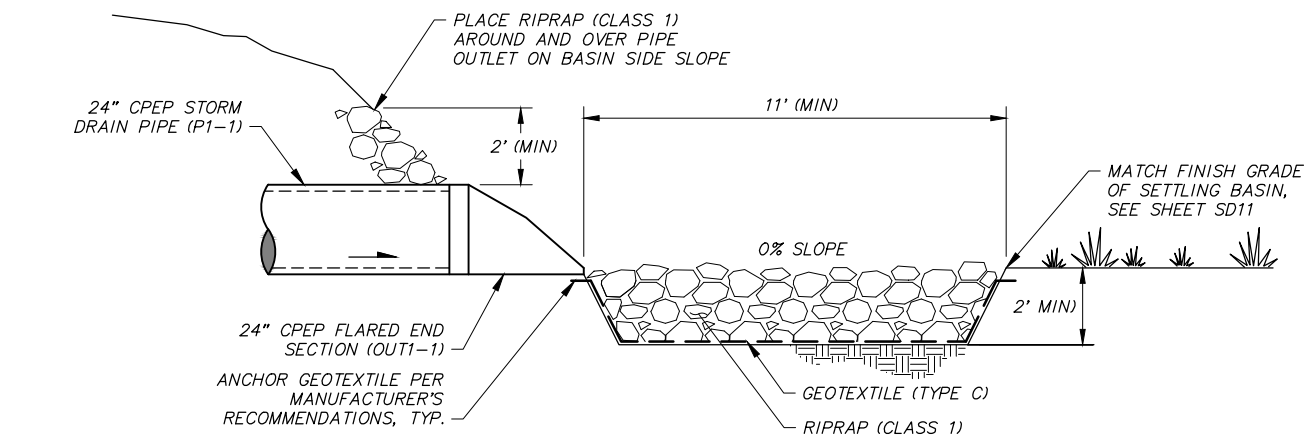
FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
	GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
STAKING							
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							
REVISIONS							

CRW ENGINEERING GROUP, LLC
3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AEC1882-AK

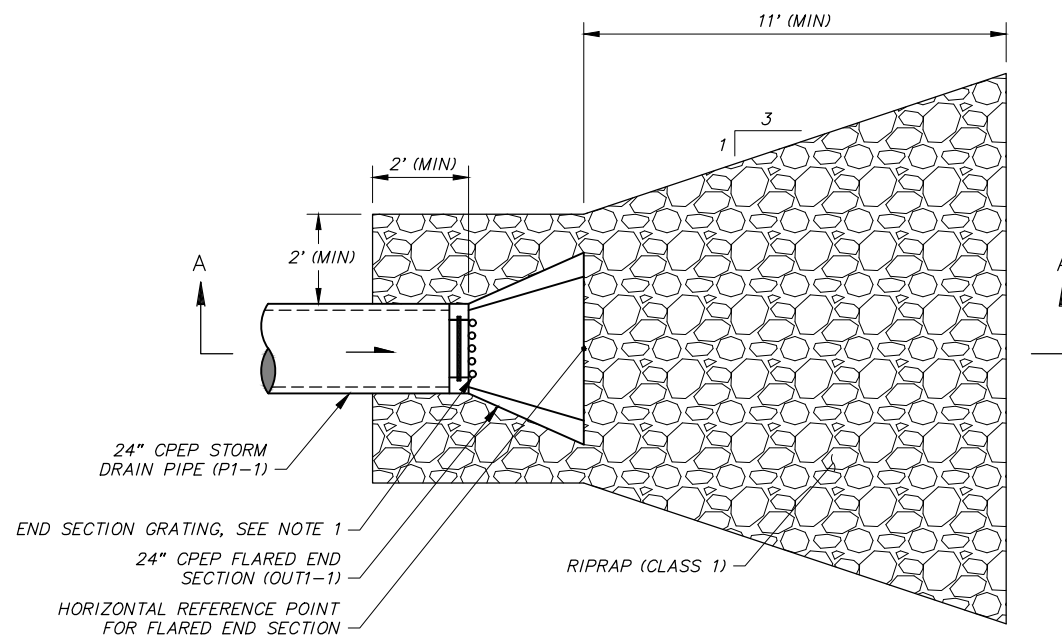
STATE OF ALASKA 49 TH Joseph C. Hegra CE-11770 REGISTERED PROFESSIONAL ENGINEER
--

MUNICIPALITY OF ANCHORAGE

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT	
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1
STORM DRAIN DETAILS	
SCALE HOR. N/A VER. N/A	GRID SW1638, SW1738 DATE JAN 2020
STATUS 95%	
SHEET SD7 of SD15	



SECTION A-A



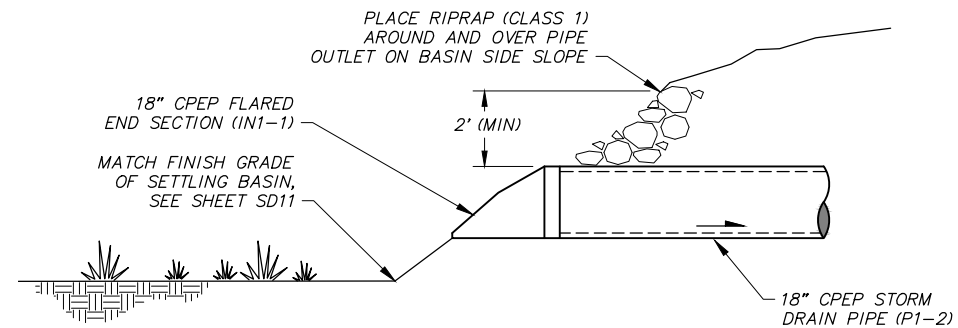
PLAN

RIPRAP APRON NOTES

1. GRATE OPENINGS SHALL BE NO MORE THAN 4-INCHES AND SHALL BE EITHER HINGED OR REMOVABLE FOR MAINTENANCE ACCESS.
2. PAYMENT FOR GEOTEXTILE (TYPE C) SHALL BE INCIDENTAL TO PAY ITEM 20.24 RIPRAP (CLASS I).
3. PAYMENT FOR FLARED END SECTION AND GRATE SHALL BE INCIDENTAL TO PAY ITEM 55.02 FURNISH AND INSTALL PIPE.

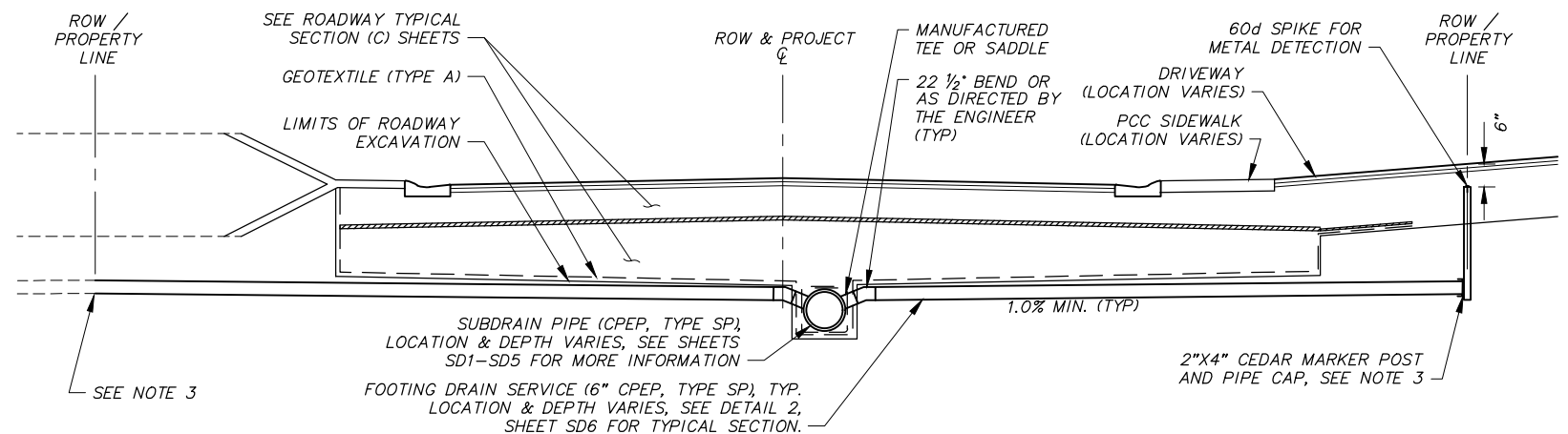
OUTFALL RIPRAP ENERGY DISSIPATOR DETAIL

SCALE: NTS



RIPRAP SLOPE PROTECTION DETAIL

SCALE: NTS






FOOTING DRAIN SERVICE NOTES

1. FINAL LOCATION OF THE FOOTING DRAIN SERVICE MAY BE ADJUSTED BY THE ENGINEER.
2. WHEN FOOTING DRAIN CONNECTS DIRECTLY TO A MANHOLE, OMIT THE 22 1/2' BEND AND CONSTRUCT THE INVERT A MINIMUM OF 1 FT ABOVE THE DOWNSTREAM INVERT.
3. CONNECT TO ON-PROPERTY FOOTING DRAIN, WHEN PRESENT, AT PROPERTY LINE, AND OMIT MARKER POST. CONTRACTOR SHALL ADAPT AND PROVIDE BELL-REDUCER OR COUPLING CONNECTION TO EXISTING FOOTING DRAIN OF WHATEVER PIPE SIZE AND TYPE AND RESOLVE CONNECTION DETAILS WITH PROPERTY OWNER AND ENGINEER.
4. CONNECTION TO EXISTING FOOTING DRAIN SHALL BE INCIDENTAL TO PAY ITEM 55.18, CONSTRUCT FOOTING DRAIN SERVICE.

FOOTING DRAIN SERVICE DETAIL

SCALE: NTS

RECORD DRAWING 1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ BY: _____ TITLE: _____ DATE: _____ 2. DATA TRANSFERRED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ 3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ BY: _____		<table border="1"> <tr> <th>DATA</th> <th>DRAWN BY</th> <th>CHECKED BY</th> </tr> <tr> <td>BASE</td> <td>TS</td> <td>M.J.</td> </tr> <tr> <td>TOPOGRAPHY</td> <td>TS</td> <td>M.J.</td> </tr> <tr> <td>PROFILE</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>STORM SEWER</td> <td>MV</td> <td>JH</td> </tr> <tr> <td>WATER/SANITARY SEWER</td> <td>MV</td> <td>JK</td> </tr> <tr> <td>GAS</td> <td>MV</td> <td>JK</td> </tr> <tr> <td>TELEPHONE</td> <td>MV</td> <td>JK</td> </tr> <tr> <td>ELECTRIC</td> <td>JH</td> <td>TK</td> </tr> <tr> <td>DESIGN</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>QUANTITIES</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>PRELIMINARY/FINAL</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>MUNICIPAL/STATE</td> <td>RB</td> <td>JK</td> </tr> </table>		DATA	DRAWN BY	CHECKED BY	BASE	TS	M.J.	TOPOGRAPHY	TS	M.J.	PROFILE	RB	JK	STORM SEWER	MV	JH	WATER/SANITARY SEWER	MV	JK	GAS	MV	JK	TELEPHONE	MV	JK	ELECTRIC	JH	TK	DESIGN	RB	JK	QUANTITIES	RB	JK	PRELIMINARY/FINAL	RB	JK	MUNICIPAL/STATE	RB	JK	<table border="1"> <tr> <th colspan="2">FIELD BOOKS</th> <th>BM NO.</th> <th>LOCATION</th> <th>ELEV.</th> <th>REV</th> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> </tr> <tr> <td colspan="2">DESIGN CRW Books 147, 148, & 151</td> <td>GAAB 66</td> <td>See page D-24 of the MOA Benchmark Book</td> <td>238.10</td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td colspan="2">STAKING</td> <td>GAAB 96</td> <td>See page D-26 of the MOA Benchmark Book</td> <td>313.83</td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td colspan="2">ASBUILT</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td colspan="2">CONTRACTOR</td> <td colspan="7">BASIS OF THIS DATUM GAAB 1972 ADJUST</td> </tr> <tr> <td colspan="2">INSPECTOR</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> </table>										FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	DESIGN CRW Books 147, 148, & 151		GAAB 66	See page D-24 of the MOA Benchmark Book	238.10					STAKING		GAAB 96	See page D-26 of the MOA Benchmark Book	313.83					ASBUILT									CONTRACTOR		BASIS OF THIS DATUM GAAB 1972 ADJUST							INSPECTOR									 3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AEC.LB2-AK						PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT 14-50 IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1 SCHED B STORM DRAIN DETAILS			
DATA	DRAWN BY	CHECKED BY																																																																																																																		
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20.26 – INSULATION BOARD (R–20) – PIPE CROSSINGS & STORM DRAIN INSULATION						
SHEET	STATION	OFFSET	WIDTH (FT)	LENGTH (FT)	AREA (SF)	COMMENTS
SD1	101+76	CL	4	68	272	STORM DRAIN
	103+31	CL	4	122	488	STORM DRAIN
SD2	42+35	1.36’ RT	4	8	32	SEWER SERVICE
	42+56	0.71’ LT	4	8	32	SEWER SERVICE
	42+85	0.46’ RT	4	8	32	SEWER SERVICE
	43+23	0.79’ RT	4	8	32	SEWER SERVICE
SD3	13+39	17.14’ RT	4	8	32	SEWER SERVICE
	13+53	17.70’ RT	4	8	32	SEWER SERVICE
SD4	15+49	16.92’ RT	4	8	32	SEWER SERVICE
	15+60	17.84’ RT	4	8	32	SEWER SERVICE
	16+29	19.90’ RT	4	8	32	SEWER SERVICE
	16+38	19.70’ RT	4	8	32	SEWER SERVICE
	16+99	15.60’ RT	4	8	32	SEWER SERVICE
	17+45	10.58’ RT	4	8	32	SEWER SERVICE
	17+62	18.80’ RT	4	8	32	SEWER SERVICE
SD5	18+16	12.78’ RT	4	8	32	SEWER SERVICE
	18+30	17.45’ RT	10	10	100	MANHOLE S5–1 (SEE SHEET SD8, DETAIL 3)
	18+44	11.91’ RT	4	8	32	SEWER MAIN AND SERVICE
	18+90	4.47’ RT	4	8	32	SEWER MAIN AND SERVICE
	19+43	20.20’ RT	4	8	32	SEWER SERVICE
	19+95	1.14’ RT	4	8	16	SEWER SERVICE

INSULATION BOARD NOTES

1. INSULATION BOARD SHALL BE INSTALLED I.A.W. TYPICAL STORM DRAIN AND SUBDRAIN TRENCH SECTIONS (DETAIL 1 & 2, SHEET SD6) AND MASS DETAIL 20–9.

50.04 – RAISE OR LOWER SEWER SERVICE						
SHEET	START STATION	START OFFSET (FT)	END STATION	END OFFSET (FT)	LENGTH (FT)	REMARKS
SD3	10+68	5.2 LT	10+58	31.8 RT	38	POTENTIAL CONFLICT WITH STORM DRAIN
SD3	11+11	1.1 RT	11+09	30.1 RT	29	POTENTIAL CONFLICT WITH STORM DRAIN
SD3	11+20	1.4 RT	11+19	30.0 RT	29	POTENTIAL CONFLICT WITH STORM DRAIN

60.05 – FURNISH AND INSTALL WATER SERVICE LINE						
SHEET	START STATION	START OFFSET (FT)	END STATION	END OFFSET (FT)	LENGTH (FT)	REMARKS
SD4	14+01	11.3 LT	14+05	25.5 RT	40	POTENTIAL CONFLICT WITH STORM DRAIN
SD5	18+35	3.1 LT	18+29	30.0 RT	34	POTENTIAL CONFLICT WITH STORM DRAIN










WATER SERVICE LINES NOTES

1. EXISTING WATER SERVICES IN CONFLICT WITH PROPOSED STORM DRAIN NEED TO BE FIELD VERIFIED FOR PROPER VERTICAL SEPARATION. RAISE OR LOWER WATER SERVICE AS NECESSARY TO MAINTAIN A MINIMUM OF 18–INCHES OF VERTICAL SEPARATION FROM PROPOSED STORM DRAIN.
2. NO THREE PART UNIONS WILL BE ALLOWED IN ROW FOR WATER SERVICE REPAIRS OR LOWERING.
3. WATER SERVICE LINE SHALL BE INSTALLED PER MASS STANDARD DETAIL 60–13.

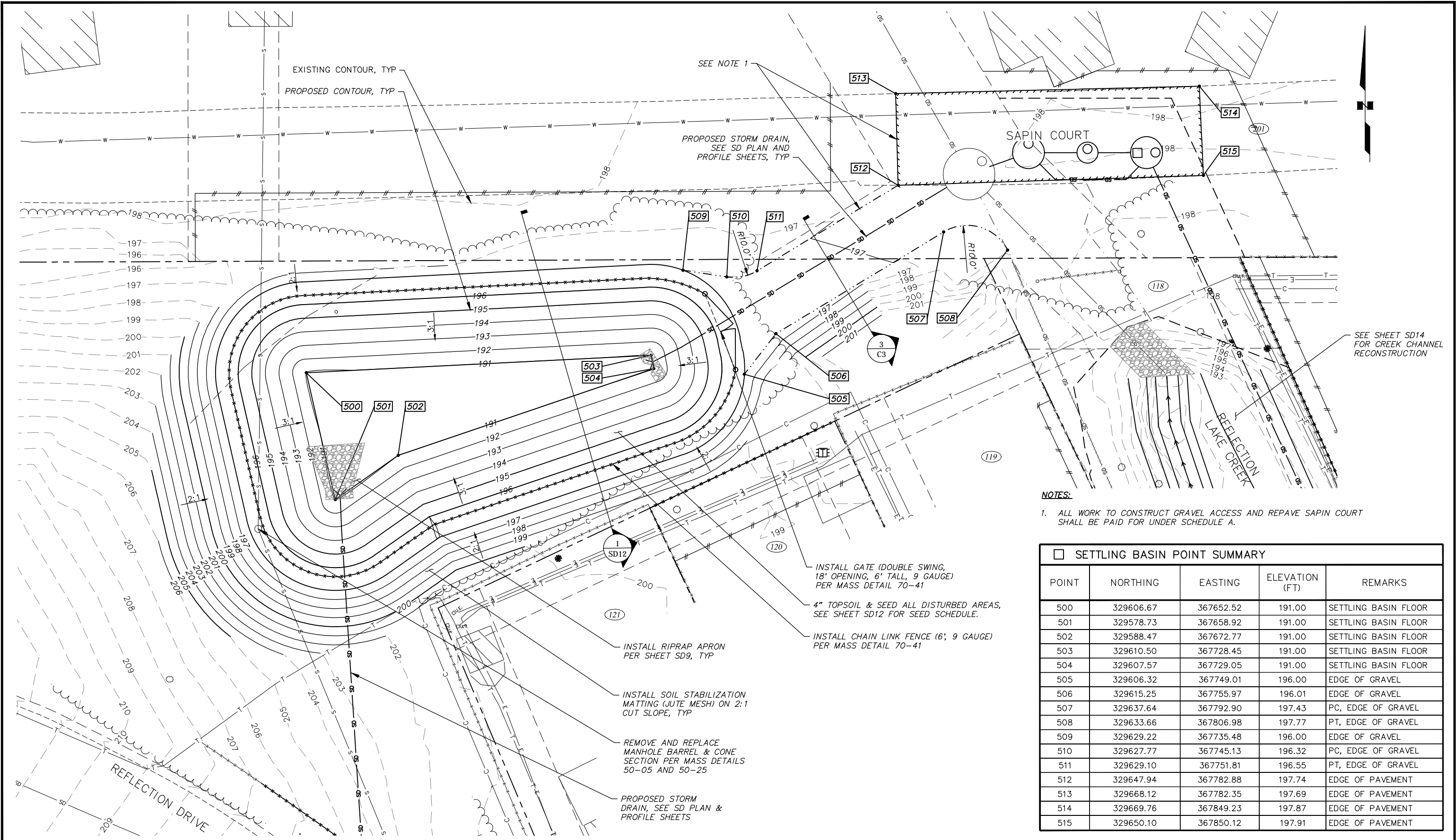
55.18 – CONSTRUCT FOOTING DRAIN SERVICE								
SHEET	PARCEL	AT MAIN		AT PROPERTY LINE		APPROX.	CONNECT TO	ELEVATION
		STATION	OFFSET (FT)	STATION	OFFSET (FT)	LENGTH (FT)		AT ROW (2)
SD2	121	40+82	30.0 LT	40+82	2.2 RT	32.2	SUBDRAIN PIPE	
	120	40+91	30.0 LT	40+91	2.2 RT	32.3	SUBDRAIN PIPE	
	119	41+72	30.0 LT	41+71	3.0 RT	31.1	MANHOLE S2–2	
	117	42+26	30.0 LT	42+18	17.4 LT	12.0	MANHOLE S2–3	
	116	42+65	30.0 LT	42+64	0.0 RT	30.0	SUBDRAIN PIPE	
	115	42+71	30.0 LT	42+71	0.3 RT	30.3	SUBDRAIN PIPE	
	114	43+32	30.6 LT	43+32	0.9 RT	31.4	SUBDRAIN PIPE	
SD3	1	9+81	38.5 RT	10+14	12.6 LT	57.7	MANHOLE S3–1	
	2	10+50	32.2 RT	10+55	5.5 RT	23.5	MANHOLE S3–2	
	3	11+01	30.2 RT	11+03	16.2 RT	14.2	SUBDRAIN PIPE	
	4	11+33	30.0 RT	11+34	17.6 RT	12.4	SUBDRAIN PIPE	
	122	11+66	30.0 LT	11+65	17.5 RT	46.2	SUBDRAIN PIPE	
	5	11+82	29.9 RT	11+81	17.5 RT	8.9	MANHOLE S3–5	
	6	11+97	30.0 RT	11+97	17.1 RT	13.1	SUBDRAIN PIPE	
	123	12+00	30.0 LT	12+08	16.8 RT	47.4	SUBDRAIN PIPE	
	124	12+42	30.0 LT	12+41	16.0 RT	45.9	SUBDRAIN PIPE	
	7	12+60	29.9 RT	12+60	15.6 RT	14.4	SUBDRAIN PIPE	
	125	12+78	30.0 LT	12+78	15.1 RT	45.1	SUBDRAIN PIPE	
	8	12+80	30.0 RT	12+80	15.1 RT	15.0	SUBDRAIN PIPE	
	126	13+22	30.0 LT	13+20	15.8 RT	46.0	SUBDRAIN PIPE	
	9	13+33	30.0 RT	13+33	16.7 RT	13.3	SUBDRAIN PIPE	
	127	13+55	30.0 LT	13+54	17.7 RT	47.7	SUBDRAIN PIPE	
	10	13+62	30.0 RT	13+63	17.9 RT	12.3	SUBDRAIN PIPE	
SD4	128	13+96	30.0 LT	13+97	17.8 RT	47.8	SUBDRAIN PIPE	
	11	14+18	30.0 RT	14+18	17.5 RT	12.7	SUBDRAIN PIPE	
	129	14+29	29.9 LT	14+33	17.2 RT	47.2	SUBDRAIN PIPE	
	130	14+57	30.0 LT	14+60	16.1 RT	46.0	SUBDRAIN PIPE	
	12	14+92	30.0 RT	14+92	13.5 RT	16.6	SUBDRAIN PIPE	
	131	14+98	29.9 LT	15+01	12.4 RT	42.5	SUBDRAIN PIPE	
	132	15+25	30.0 LT	15+19	13.6 RT	44.1	SUBDRAIN PIPE	
	13	15+36	30.0 RT	15+37	15.8 RT	14.2	SUBDRAIN PIPE	
	133	15+67	29.9 LT	15+65	18.2 RT	48.2	SUBDRAIN PIPE	
	14	15+86	30.0 RT	15+87	19.4 RT	10.9	SUBDRAIN PIPE	
	134	16+00	30.0 LT	15+99	19.7 RT	49.6	SUBDRAIN PIPE	
	15	16+05	30.1 RT	16+05	19.9 RT	10.2	SUBDRAIN PIPE	
	135	16+30	30.0 LT	16+31	19.8 RT	49.7	SUBDRAIN PIPE	
	16	16+48	30.0 RT	16+48	19.4 RT	10.8	SUBDRAIN PIPE	
	136	16+65	30.0 LT	16+69	18.2 RT	48.3	SUBDRAIN PIPE	
	17	16+90	30.0 RT	16+89	16.7 RT	13.6	SUBDRAIN PIPE	
	137	17+06	29.9 LT	17+14	13.8 RT	44.4	SUBDRAIN PIPE	
	18	17+33	30.0 RT	17+31	11.8 RT	18.4	SUBDRAIN PIPE	
	138	17+39	30.0 LT	17+46	10.4 RT	41.0	SUBDRAIN PIPE	
	19	17+69	30.0 RT	17+65	8.5 RT	22.1	SUBDRAIN PIPE	
SD5	139	17+84	30.0 LT	17+86	8.0 RT	37.9	SUBDRAIN PIPE	
	20	18+04	30.0 RT	18+07	10.5 RT	19.8	SUBDRAIN PIPE	
	21	18+45	30.0 RT	18+42	12.3 RT	18.1	SUBDRAIN PIPE	
	22	18+55	30.0 RT	18+54	8.9 RT	21.2	SUBDRAIN PIPE	
	23	19+01	30.0 RT	19+03	5.1 RT	25.3	SUBDRAIN PIPE	
	140	19+52	30.0 LT	19+61	1.9 RT	32.9	SUBDRAIN PIPE	

FOOTING DRAIN SERVICE NOTES

1. FOOTING DRAIN SERVICES SHALL BE INSTALLED PER DETAIL 3, SHEET SD9 AND THE SPECIAL PROVISIONS.
2. TO BE COMPLETED BY CONTRACTOR AS PART OF AS–BUILT DRAWINGS.
3. FOOTING DRAIN SERVICES SHALL BE INSTALLED A MINIMUM OF 11 FEET FROM ANY WATER SERVICE KEY BOX.
4. FOOTING DRAIN SERVICES SHALL BE CONSTRUCTED I.A.W. TYPICAL SUBDRAIN TRENCH SECTION (DETAIL 2, SHEET SD6).

RECORD DRAWING 1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ BY: _____ TITLE: _____ DATE: _____		2. DATA TRANSFERRED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____		3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR–PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ BY: _____		<table><tr><td>DATA</td><td>DRAWN BY</td><td>CHECKED BY</td></tr><tr><td>BASE</td><td>TS</td><td>MJ</td></tr><tr><td>TOPOGRAPHY</td><td>TS</td><td>MJ</td></tr><tr><td>PROFILE</td><td>RB</td><td>JK</td></tr><tr><td>STORM SEWER</td><td>MV</td><td>JH</td></tr><tr><td>WATER/SANITARY SEWER</td><td>MV</td><td>JK</td></tr><tr><td>GAS</td><td>MV</td><td>JK</td></tr><tr><td>TELEPHONE</td><td>MV</td><td>JK</td></tr><tr><td>ELECTRIC</td><td>JH</td><td>TK</td></tr><tr><td>DESIGN</td><td>RB</td><td>JK</td></tr><tr><td>QUANTITIES</td><td>RB</td><td>JK</td></tr><tr><td>PRELIMINARY/FINAL</td><td>RB</td><td>JK</td></tr><tr><td>MUNICIPAL/STATE</td><td>RB</td><td>JK</td></tr></table>	DATA	DRAWN BY	CHECKED BY	BASE	TS	MJ	TOPOGRAPHY	TS	MJ	PROFILE	RB	JK	STORM SEWER	MV	JH	WATER/SANITARY SEWER	MV	JK	GAS	MV	JK	TELEPHONE	MV	JK	ELECTRIC	JH	TK	DESIGN	RB	JK	QUANTITIES	RB	JK	PRELIMINARY/FINAL	RB	JK	MUNICIPAL/STATE	RB	JK	<table><tr><td>FIELD BOOKS</td><td>BM NO.</td><td>LOCATION</td><td>ELEV.</td><td>REV</td><td>DATE</td><td>DESCRIPTION</td><td>BY</td></tr><tr><td>DESIGN CRW Books 147, 148, & 151</td><td>GAAB 66</td><td>See page D–24 of the MOA Benchmark Book</td><td>238.10</td><td></td><td></td><td></td><td></td></tr><tr><td></td><td>GAAB 96</td><td>See page D–26 of the MOA Benchmark Book</td><td>313.83</td><td></td><td></td><td></td><td></td></tr><tr><td>STAKING</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>ASBUILT</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>CONTRACTOR</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>INSPECTOR</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr></table>	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D–24 of the MOA Benchmark Book	238.10						GAAB 96	See page D–26 of the MOA Benchmark Book	313.83					STAKING								ASBUILT								CONTRACTOR								INSPECTOR																<table><tr><td>PLAN CHECK</td><td>CONSTRUCTION RECORD</td><td>VERTICAL DATUM</td><td>REVISIONS</td><td>CONSULTANT</td><td>SEAL</td></tr></table>	PLAN CHECK	CONSTRUCTION RECORD	VERTICAL DATUM	REVISIONS	CONSULTANT	SEAL	<table><tr><td colspan="2"> 3940 ARCTIC BLVD, SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562–3252 #AEC0882–AK</td><td> 49 TH Joseph C. Hegra CE-11770 REGISTERED PROFESSIONAL ENGINEER</td><td></td></tr></table>	 3940 ARCTIC BLVD, SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562–3252 #AEC0882–AK		 49 TH Joseph C. Hegra CE-11770 REGISTERED PROFESSIONAL ENGINEER		<table><tr><th colspan="4">PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT</th></tr><tr><td>14–50</td><td>IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION – PHASE 1</td><td>SCHED B</td><td></td></tr><tr><td colspan="4">STORM DRAIN SUMMARY TABLES</td></tr><tr><td>SCALE</td><td>HOR. N/A VER. N/A</td><td>GRID SW1638, SW1738 DATE JAN 2020</td><td>STATUS 95%</td></tr><tr><td colspan="2">SHEET</td><td>SD10 of SD15</td><td></td></tr></table>	PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT				14–50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION – PHASE 1	SCHED B		STORM DRAIN SUMMARY TABLES				SCALE	HOR. N/A VER. N/A	GRID SW1638, SW1738 DATE JAN 2020	STATUS 95%	SHEET		SD10 of SD15	
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File: I:\JobData\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\01 Civil\Phase 1\10133.00 Detention Basin.dwg



□ SETTLING BASIN POINT SUMMARY				
POINT	NORTHING	EASTING	ELEVATION (FT)	REMARKS
500	329606.67	367652.52	191.00	SETTLING BASIN FLOOR
501	329578.73	367658.92	191.00	SETTLING BASIN FLOOR
502	329588.47	367672.77	191.00	SETTLING BASIN FLOOR
503	329610.50	367728.45	191.00	SETTLING BASIN FLOOR
504	329607.57	367729.05	191.00	SETTLING BASIN FLOOR
505	329606.32	367749.01	196.00	EDGE OF GRAVEL
506	329615.25	367755.97	196.01	EDGE OF GRAVEL
507	329637.64	367792.90	197.43	PC, EDGE OF GRAVEL
508	329633.66	367806.98	197.77	PT, EDGE OF GRAVEL
509	329629.22	367735.48	196.00	EDGE OF GRAVEL
510	329627.77	367745.13	196.32	PC, EDGE OF GRAVEL
511	329629.10	367751.81	196.55	PT, EDGE OF GRAVEL
512	329647.94	367782.88	197.74	EDGE OF PAVEMENT
513	329668.12	367782.35	197.69	EDGE OF PAVEMENT
514	329669.76	367849.23	197.87	EDGE OF PAVEMENT
515	329650.10	367850.12	197.91	EDGE OF PAVEMENT

RECORD DRAWING

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BY: _____ TITLE: _____ DATE: _____

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COMPANY: _____ DATE: _____

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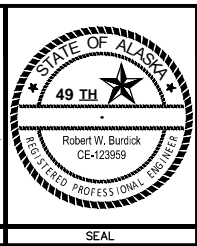
DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
	GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				

STAKING							
ASBUILT							
CONTRACTOR							
INSPECTOR							

BASIS OF THIS DATUM GAAB 1972 ADJUST

PLAN CHECK	CONSTRUCTION RECORD	VERTICAL DATUM	REVISIONS	CONSULTANT	SEAL
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PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

14-50 IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1 SCHED B

SETTLING BASIN SITE & GRADING PLAN

SCALE HOR. 1"=10' VER. N/A GRID SW1638, SW1738 DATE JAN 2020 STATUS 95% SHEET SD11 of SD15

1. PLACE 4" OF TOPSOIL AND SEEDING (SCHEDULE C - WETLANDS MIX) ON ALL DISTURBED AREAS BELOW ELEVATION 196.0'. ALL OTHER DISTURBED AREAS SHALL RECEIVE 4" TOPSOIL AND SEEDING (SCHEDULE B).



CHAIN LINK FENCE NOTES:

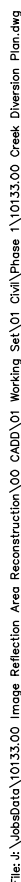
1. SEE MASS DETAIL 70-41 FOR CHAIN LINK FENCE DETAILS.
2. STAKE FENCE LAYOUT IN FIELD FOR ENGINEER TO REVIEW AND APPROVE PRIOR TO INSTALLATION. THIS WORK SHALL BE INCIDENTAL TO SECTION 70.18 PAY ITEM.

DOUBLE SWING GATE NOTES:

1. SEE MASS DETAIL 70-41 FOR DOUBLE SWING GATE DETAILS.
2. STAKE DOUBLE SWING GATE LAYOUT IN FIELD FOR ENGINEER TO REVIEW AND APPROVE PRIOR TO INSTALLATION. THIS WORK SHALL BE INCIDENTAL TO SECTION 70.18 PAY ITEM.

File: J:\JobsData\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\01 Civil\Phase 1\10133.00 Detention Basin.dwg

1. DIVERSION STRUCTURE LOCATIONS ARE APPROXIMATE. INSTALL ADDITIONAL DIVERSION STRUCTURES AS REQUIRED TO MAINTAIN DIVERTED FLOWS FROM ENTERING THE WORK AREA. THE HEIGHT OF DIVERSION STRUCTURES SHALL BE DETERMINED/ADJUSTED AS NEEDED TO MAINTAIN DIVERTED FLOWS.
2. CONTRACTOR SHALL REPAIR ANY DAMAGE RESULTING FROM THE DIVERSION.
3. REFER TO SPECIFICATIONS FOR ADDITIONAL CREEK DIVERSION REQUIREMENTS.
4. STREAM DIVERSION ACTIVITIES SHALL ADHERE TO REQUIREMENTS IN ALL APPLICABLE PERMITS.

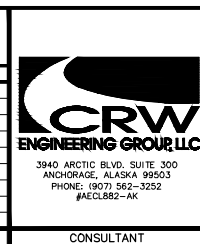


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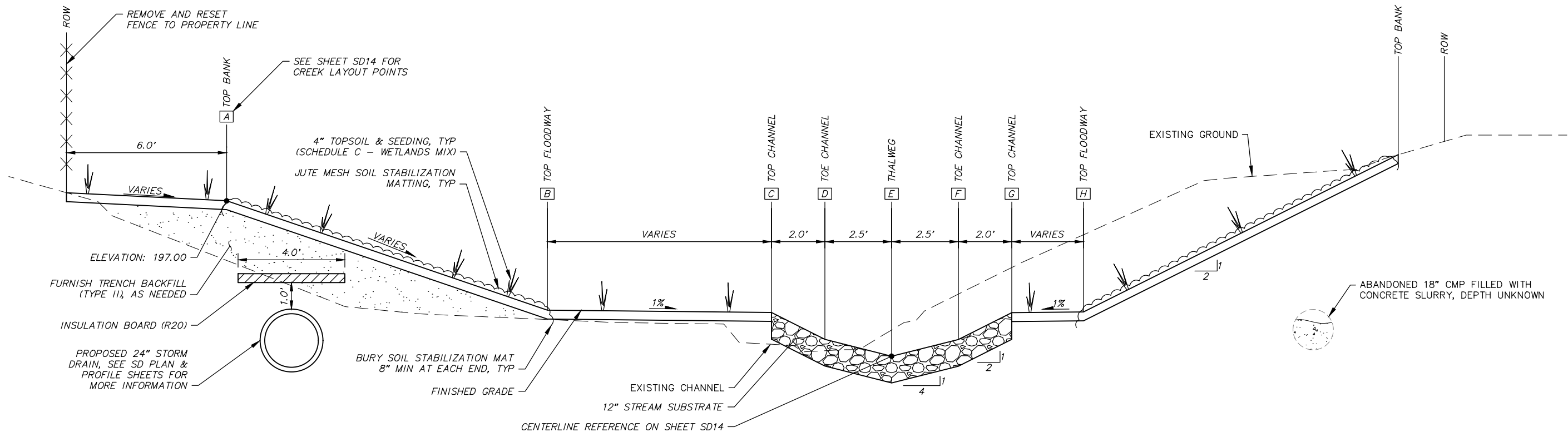
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<div><div>201001020</div><div>GRAPHIC</div><div>SCALE</div></div>								
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DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10					
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STAKING								
ASBUILT								
CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST							
INSPECTOR								
CONSTRUCTION RECORD		VERTICAL DATUM			REVISIONS			



14-50		IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1		SCHD B	
CREEK DIVERSION PLAN					
SCALE HOR. 1"=10' VER. N/A		GRID SW1638, SW1738 DATE JAN 2020 STATUS 95%		SD13 SHEET of SD15	

File: I:\JobData\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\01 Civil\Phase 1\10133.00 Typical Section - Creek.dwg



1 **CREEK RECONSTRUCTION TYPICAL SECTION**
SCALE: NTS

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TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
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ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							

CRW
ENGINEERING GROUP LLC

3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECLE882-AK

STATE OF ALASKA
49 TH
Robert W. Burdick
CE-123959
REGISTERED PROFESSIONAL ENGINEER




PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1		SCHED B
CREEK RECONSTRUCTION TYPICAL SECTION			
SCALE HOR. N/A VER. N/A	GRID SW1638, SW1738 DATE JAN 2020	STATUS 95%	SD15 of SD15

LIGHT LEVELS TABLE						
LOCATION	MOA REQUIRED MIN. AVERAGE ILLUMINANCE (FC)	AVERAGE DESIGN ILLUMINANCE (FC)	MOA REQUIRED MAXIMUM UNIFORMITY RATIO	DESIGN UNIFORMITY RATIO	MOA REQUIRED MAX. VEILING LUMINANCE RATIO	DESIGN VEILING LUMINANCE RATIO
IMAGE DRIVE	0.4	0.5	6.0:1	2.4:1	0.3:1	0.3:1
REFLECTION DRIVE	0.4	0.7	6.0:1	3.6:1	0.3:1	0.3:1
IMAGE DRIVE/REFLECTION DRIVE INTX – NORTH	0.8	1.4	6.0:1	2.3:1	N/A	N/A
IMAGE DRIVE/MIRAGE CIRCLE INTX	0.8	1.2	6.0:1	2.3:1	N/A	N/A
REFLECTION DRIVE/LOON COVE CIRCLE INTX	0.8	1.2	6.0:1	1.4:1	N/A	N/A
PEDESTRIAN FACILITIES	0.4	0.4	4.0:1	3.4:1	N/A	N/A

NOTES:

1. MOA REQUIREMENTS ARE FROM 2007 DCM CHAPTER 5 FOR A LOCAL ROADWAY WITH LOW PEDESTRIAN CONFLICT (MEDIUM DENSITY RESIDENTIAL).
2. LIGHT LOSS FACTOR (LLF) = 0.93.
3. MOUNTING HEIGHTS ARE 30'.

LUMINAIRE DEFINITION											
TYPE	SYMBOL	MAKE	MODEL	LUMENS	CCT*	DISTRIBUTION	VOLTAGE	DRIVE CURRENT	COLOR	OPTIONS	MOUNT
ROADWAY		CREE	LEDWAY	SEE LUMINAIRE SCHEDULE	4000K	MEDIUM, TYPE 2	240	525mA	SILVER	7-PIN RECEPTACLE, BACKLIGHT SHIELD	MAST ARM

*CCT = CORRELATED COLOR TEMPERATURE

ROADWAY LUMINAIRE SCHEDULE							
POLE	STATION	OFFSET	SHAFT LENGTH	MAST ARM LENGTH	LUMENS	DISTRIBUTION	CIRCUIT
L1	43+45.2	30.81 LT	26'	14'	12,000	MEDIUM, TYPE 2	T3
L2	41+83.1	27.43 LT	26'	10'	9,000	MEDIUM, TYPE 2	T1
L3	09+99.2	22.86 LT	28'	8'	6,000	MEDIUM, TYPE 2	T1
L4	11+38.5	31.76 LT	27'	11'*	6,000	MEDIUM, TYPE 2	T4
				17'	9,000	MEDIUM, TYPE 2	
L5	13+00.5	22.85 LT	28'	8'	9,000	MEDIUM, TYPE 2	T4
L6	14+80.4	24.37 LT	28'	9'	15,000	MEDIUM, TYPE 2	T4
L7	16+84.9	23.60 LT	29'	6'	12,000	MEDIUM, TYPE 2	T4
L8	18+37.0	27.32 LT	28'	9'	6,000	MEDIUM, TYPE 2	T4
L9	19+48.7	26.59 LT	28'	9'	9,000	MEDIUM, TYPE 2	T4
NOTES:							
[1]	* = NORTH						
[2]	ALL LUMINAIRE PILE FOUNDATIONS SHALL HAVE A MINIMUM EMBEDMENT DEPTH OF 25'.						
[3]	PROVIDE A SPARE POLE WITH THE SAME CHARACTERISTICS AS POLE L4.						

EXISTING ROADWAY LUMINAIRE SCHEDULE			
POLE	STATION	OFFSET	CIRCUIT
EL1	44+88.6	21.43 LT	T5
EL2	63+06.0	19.25 RT	T3
EL3	08+48.8	20.53 LT	T1
EL4	30+81.9	14.34 RT	T4
EL5	21+70.5	23.20 LT	T6

JUNCTION BOX SCHEDULE				
J-BOX	TYPE	CIRCUIT	STATION	OFFSET
J2A	1A	T1,T4	10+87.5	26.21 LT
J9A	1A	T4	30+38.9	21.66 RT
NOTE: ONLY JUNCTION BOXES NOT ASSOCIATED WITH A LUMINAIRE OR LOAD CENTER ARE SHOWN IN THIS TABLE.				

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GAS	MV	JK	STAKING									
TELEPHONE	MV	JK										
ELECTRIC	JH	TK	ASBUILT									
DESIGN	RB	JK										
QUANTITIES	RB	JK	CONTRACTOR		BASIS OF THIS DATUM GAAB 1972 ADJUST							
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MUNICIPAL/STATE	RB	JK	INSPECTOR									
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CRW
ENGINEERING GROUP, LLC

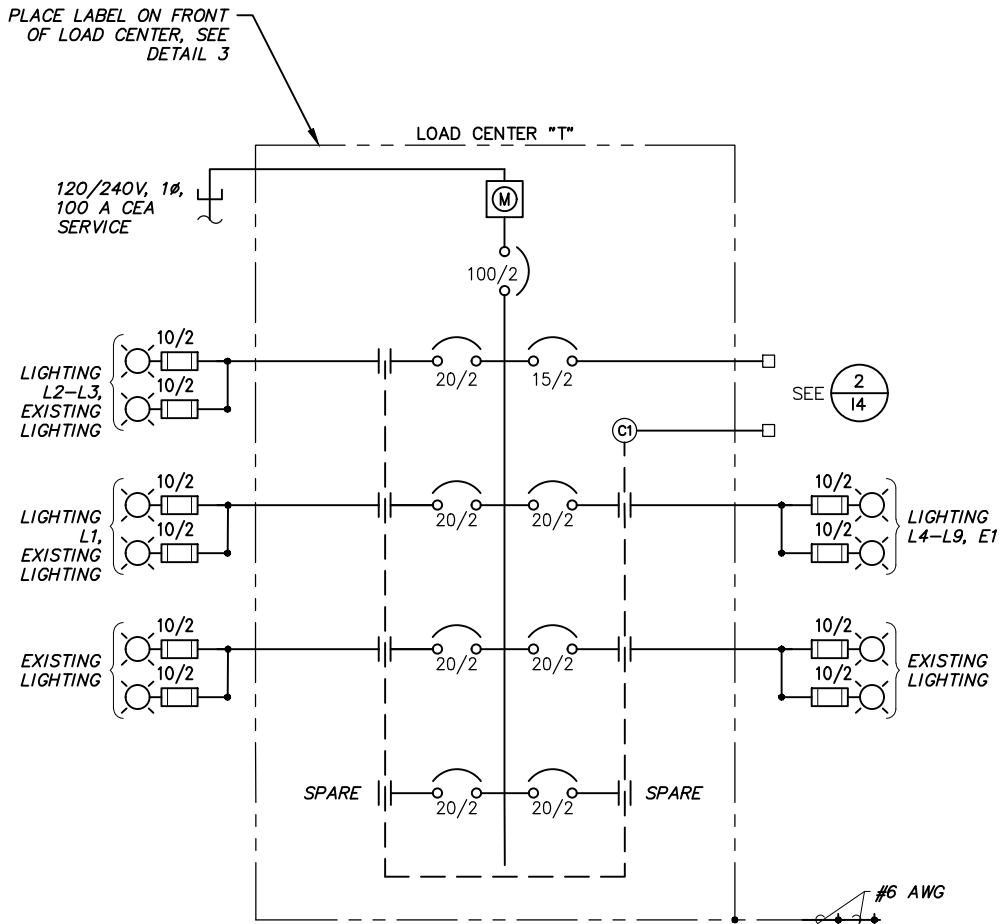
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PHONE: (907) 562-3252
#AECL882-AK

CONSULTANT

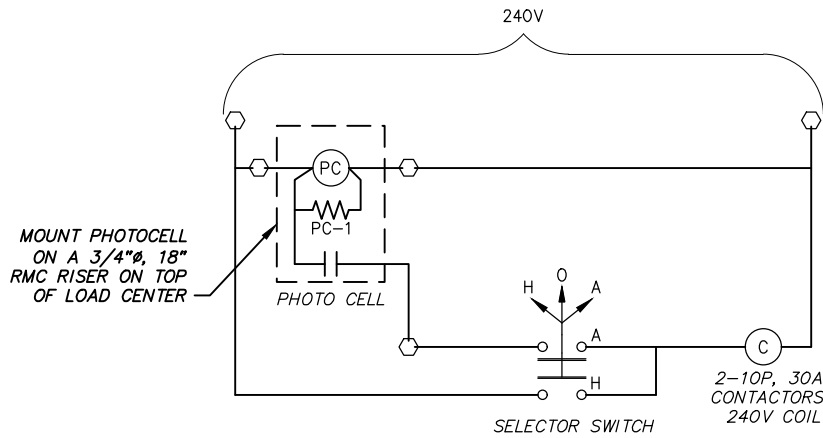


PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50		IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	
		SCHED C	
ILLUMINATION SCHEDULES			
SCALE		GRID SW1638, SW1738	
HOR. N/A VER. N/A		DATE JAN 2020	STATUS 95%
		SHEET 13 of 14	

File: I:\JobData\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\03 Electrical\Phase 1\10133.00 Illumination - Load Center Details.dwg



1 LOAD CENTER "T" POWER ONE-LINE
NTS



2 LOAD CENTER LIGHTING CONTROL SCHEMATIC
NTS

LOAD CENTER NO. T TYPE: 1A

LOCATION: STATION - 42+09, OFFSET - 27.3 LT, IMAGE DRIVE

2-10 POLE, 30 AMP CONTACTORS

MAIN BREAKER A: 2 POLE, 100 AMPS, 240 VOLTS

PANEL A 100 AMPS MAIN LUGS, 120/240 VOLTS SINGLE PHASE 3 WIRE

10,000 AMPS INTERRUPT CAPACITY

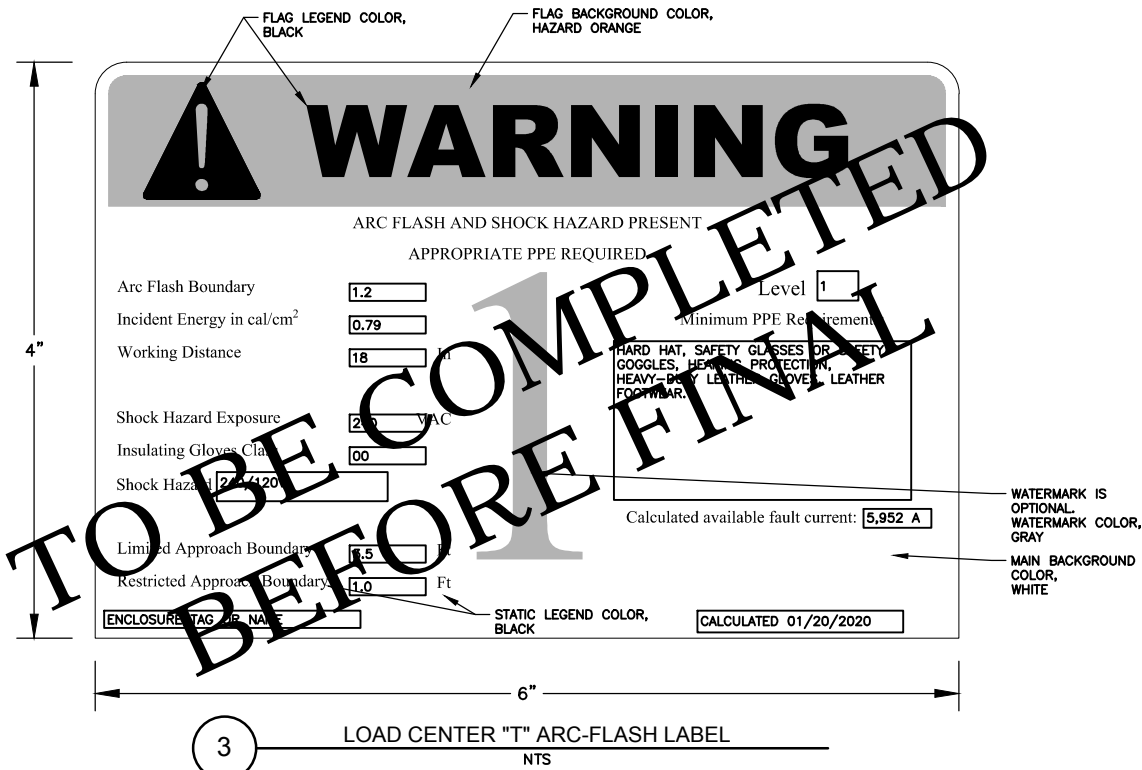
CKT.	CIRCUIT DESCRIPTION	KVA	AMP		AMP	KVA	CIRCUIT DESCRIPTION	CKT.
T1	ELECTROLIERS L2-L3, EXISTING ELECTROLIERS	0.4	20/2	1	15/2	0.2	PHOTOELECTRIC CONTROL	T2
T3	ELECTROLIER L1, EXISTING ELECTROLIERS	0.3	20/2	3	20/2	0.8	ELECTROLIERS L4-L9, E1	T4
T5	EXISTING ELECTROLIERS	0.8	20/2	5	20/2	0.3	EXISTING ELECTROLIERS	T6
T7	SPARE		20/2	7	20/2		SPARE	
				9				
				11				
				13				
				15				
				17				
				19				

TOTAL CONNECTED LOAD = 2.8 KVA

TOTAL AMPS = 11.8 A

VOLTAGE DROPS					
CIRCUIT	SIZE	LENGTH	VOLTAGE	CURRENT	V.D.
T1	#8 AWG	446	240V	1.58	0.43%
T3	#8 AWG	440	240V	1.2	0.32%
T4	#8 AWG	1047	240V	3.51	2.23%
T5	#8 AWG	1748	240V	3.19	3.38%
T6	#8 AWG	2039	240V	1.45	1.79%

- LOAD CENTER NOTES:
1. LABELS FOR LOAD CENTERS SHALL HAVE SUFFICIENT DURABILITY TO WITHSTAND THE ENVIRONMENT INVOLVED. CONTACT ENGINEER PRIOR TO ORDER OF PLACARD TO VERIFY MAXIMUM FAULT CURRENT.
 2. LABEL THE FRONT WITH 3M SCOTCH CAL REFLECTIVE DECALS NOTING OWNERSHIP: MOA, PURPOSE: LU (ILLUMINATION) AND THE VOLTAGE.



RECORD DRAWING

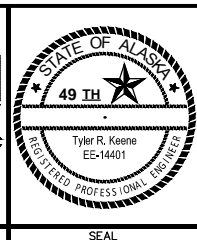
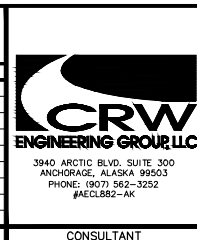
1. DATA PROVIDED BY: TITLE: THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: BY: DATE:

2. DATA TRANSFERRED BY: TITLE: COMPANY: DATE:

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: TITLE: COMPANY: DATE: BY:

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
	GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
STAKING							
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							
VERTICAL DATUM							
REVISIONS							



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

14-50 IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1 SCHED C

POWER ONE-LINE, PANEL SCHEDULE, AND CONTROL SCHEMATIC

SCALE HOR. N/A VER. N/A GRID SW1638, SW1738 DATE JAN 2020 STATUS 95% SHEET 14 of 14

File: I:\JobData\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\03 Electrical\Phase 1\10133.00 Heat Traces - Phase 1_V2.dwg

NOTES:
1. SEE SHEET E3 FOR HEAT TRACE NOTES.

DISCONNECT AND REMOVE CONDUCTORS, PRESERVE SECTION OF CONDUIT TO BE REUSED. SEE E2 FOR CONDUIT TO BE INTERCEPTED AND REUSED. IF NOT REUSED REMOVE.

REMOVE EXISTING LOAD CENTER AND HEAT TRACE CONTROLLER. NEW LOAD CENTER WITH HEAT TRACE CONTROL TO BE INSTALLED AT SAME LOCATION.

EXISTING HEAT TRACE TO REMAIN

EXISTING HEAT TRACE JUNCTION BOXES AND POSTS TO REMAIN

EXISTING HEAT TRACE POSTS TO REMAIN

IMAGE DRIVE

ELECTRICAL PLAN LEGEND

EXISTING	PROPOSED	
---	---	UNDERGROUND CONDUIT
~~~~~	~~~~~	HEAT TRACE
⊙	⊙	ABOVE GRADE JUNCTION BOX
□	■	ELECTRIC JB TYPE IA
▣	▣	ELECTRIC JB TYPE II
⊗	⊗	LOAD CENTER
□	□	HEAT TRACE END KIT

**NEW CONDUIT/CONDUCTOR TAG**

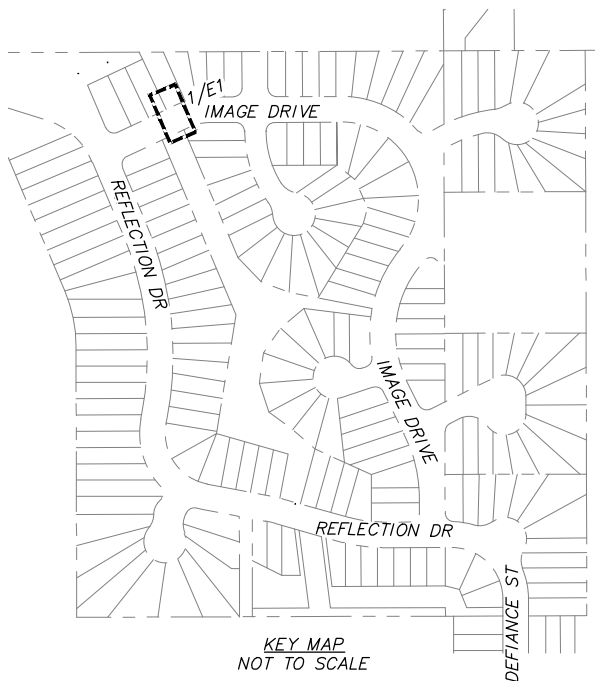
CONDUIT SIZE: 2  
# OF CABLES: 1  
# OF CONDUCTORS PER CABLE: 3C10(LTG X1)  
CIRCUIT #: 1  
TYPE OF CIRCUIT: X1  
SIZE OF CONDUCTORS: 10

**EXISTING CONDUIT/NEW CONDUCTOR TAG**

CONDUIT SIZE: 2  
# OF CABLES: 1  
# OF CONDUCTORS PER CABLE: 3C10(LTG X1)  
CIRCUIT #: 1  
TYPE OF CIRCUIT: X1  
SIZE OF CONDUCTORS: 10

## HEAT TRACE DEMOLITION PLAN

SCALE: SEE GRAPHIC SCALE



**RECORD DRAWING**

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CONTRACTOR: _____ DATE: _____  
BY: _____

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COMPANY: _____ DATE: _____  
BY: _____

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TOPOGRAPHY	TS	MJ
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WATER/SANITARY SEWER	MV	JK
GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
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FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW Books 147, 148, & 151	GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
	GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
STAKING							
ASBUILT							
CONTRACTOR							
INSPECTOR							
		BASIS OF THIS DATUM GAAB 1972 ADJUST					

**CRW ENGINEERING GROUP, LLC**

3940 ARCTIC BLVD. SUITE 300  
ANCHORAGE, ALASKA 99503  
PHONE: (907) 562-3252  
#AECLE882-AK

STATE OF ALASKA  
49 TH  
Tyler R. Keene  
EE-14401  
REGISTERED PROFESSIONAL ENGINEER



**PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT**

14-50 IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1 SCHED B

**HEAT TRACE DEMOLITION PLAN**

SCALE HOR. 1"=10' VER. N/A GRID SW1638, SW1738 DATE JAN 2020 STATUS 95% SHEET E1 of E6



File: J:\JobsData\10133.00 Image Reflection Area Reconstruction\00 CADD\01 Working Set\03 Electrical\Phase 1\10133.00 HT Controls.dwg

- 
- 120/240V  
FROM CEA
- TYPE 1A LOAD  
CENTER
- M
- 1 MAIN BREAKER
- 2
- 3
- 4
- 5
- 6
- 7
- 8
- 9
- 10
- 11

1 **LOAD CENTER CONTROL SCHEMATIC**

TYPE 1A LOAD CENTER  
SEE MASS DETAIL 80-2

PILOT LIGHT

HEAT TRACE

PLACE LABEL ON FRONT OF LOAD CENTER. SEE DETAIL 3/E6

TYPE II JUNCTION BOX

WARNING TAPE

WARNING TAPE

RMC WITH COLD LEADS TO JUNCTION BOX SIZE AS SHOWN ON DRAWINGS

30" MIN

12"  $\pm$  3"

FINISHED GRADE


4

## 2 HEAT TRACE LOAD CENTER

[illegible]



DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	TS	MJ
PROFILE	RB	JK
STORM SEWER	MV	JH
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GAS	MV	JK
TELEPHONE	MV	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
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PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK



**CRW**  
ENGINEERING GROUP, LLC

3940 ARCTIC BLVD, SUITE 300  
ANCHORAGE, ALASKA 99503  
PHONE: (907) 562-3252  
#AECL882 - AK

CONSULTANT



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1		SCHED B
HEAT TRACE DETAILS			
SCALE	HOR. N/A VER. N/A	GRID SW1638, SW1738 DATE JAN 2020	E4 of E6 SHEET 95%



- 1 INSTALL RIGID METAL CONDUITS (RMC) A MINIMUM OF 30 INCHES BELOW FINISHED GRADE.
- 2 CORE DRILL SEPARATE CONDUIT ACCESS HOLES FOR EACH CONDUIT THROUGH THE MANHOLE WALL AND GROUT AROUND THE INSTALLED CONDUIT.
- 3 PROVIDE 1-1/2" LIQUID-TIGHT FLEXIBLE METAL CONDUITS (LFMC) INSIDE THE MANHOLE. PROVIDE LFMC FITTINGS ON BOTH ENDS OF ALL SEGMENTS OF LFMC.
- 4 PROVIDE THE LFMC WITH A STAINLESS STEEL GROUND STRAP BETWEEN THE SECTIONS OF RMC.
- 5 ATTACH LFMC TO MANHOLE WALL ON 2.5' CENTERS.
- 6 PROVIDE 1-1/2" RMC WITH HEAT TRACE IN THE STORM DRAIN PIPES, EXTENDING THEM TO THE LOCATION(S) SHOWN IN THE DRAWINGS.
- 7 PROVIDE TYPE 2 OR TYPE 1A JUNCTION BOXES WITH GROUND ROD, GROUNDING BUSHINGS, AND GROUNDING PER MASS DETAIL 80-32 OR 80-31 AS SHOWN ON THE PLANS.
- 8 COMPLETE SPLICES BETWEEN HOT AND COLD LEADS IN THE JUNCTION BOX AND PROVIDE SEALING FITTINGS ON THE RMC.

**RECORD DRAWING**

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CONTRACTOR: _____

BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

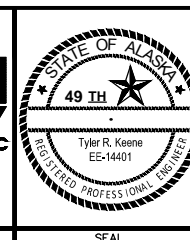
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PROFILE	RB	JH	FIELD BOOKS				BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
STORM SEWER	MV	JK	DESIGN CRW Books 147, 148, & 151				GAAB 66	See page D-24 of the MOA Benchmark Book	238.10				
WATER/SANITARY SEWER	MV	JK					GAAB 96	See page D-26 of the MOA Benchmark Book	313.83				
GAS	MV	JK	STAKING										
TELEPHONE	MV	JK											
ELECTRIC	JH	TK											
DESIGN	RB	JK					ASBUILT						
QUANTITIES	RB	JK					CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST					
PRELIMINARY/FINAL	RB	JK					INSPECTOR						
MUNICIPAL/STATE	RB	JK											
PLAN CHECK			CONSTRUCTION RECORD				VERTICAL DATUM			REVISIONS			



<b>PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT</b>			
14-50	IMAGE DRIVE / REFLECTION DRIVE AREA ROAD RECONSTRUCTION - PHASE 1	SCHED B	
<h2 style="margin: 0;">MANHOLE HEAT TRACE DETAILS</h2>			
SCALE    HOR. N/A VER. N/A		GRID SW1636, SW1736 DATE JAN 2020    STATUS 95%	SHEET <span style="font-size: 1.5em;">E5</span> of <span style="font-size: 1.5em;">E6</span>

